

STATE OF NEW HAMPSHIRE

Department of Administrative Services

DIVISION OF PUBLIC WORKS DESIGN & CONSTRUCTION

CONCORD AVIATION READINESS CENTER

26 Regional Drive, Concord, New Hampshire

DPW Project #81018R Contract: A

Department of Military Affairs and Veteran
Services - NH National Guard



7 Hazen Drive PO Box 483
Room 250

Concord, New Hampshire 03301
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COMMISSIONER-DEPARTMENT OF ADMINISTRATIVE SERVICES

SIGNATURE DATE

ADMINISTRATOR-DIVISION OF PUBLIC WORKS

SIGNATURE DATE

Adjutant General

SIGNATURE DATE

LOCUS MAP

REVISIONS

DATE SYMBOL

DESCRIPTION

DATE SYMBOL

DESCRIPTION

DATE SYMBOL

DESCRIPTION

PROJECT NAME
CONCORD AVIATION
READINESS CENTER

ISSUE DATE

JULY 1, 2020

SHEET NUMBER

G000

CIVIL



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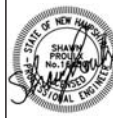
MECHANICAL



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ELECTRICAL



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REFERENCES

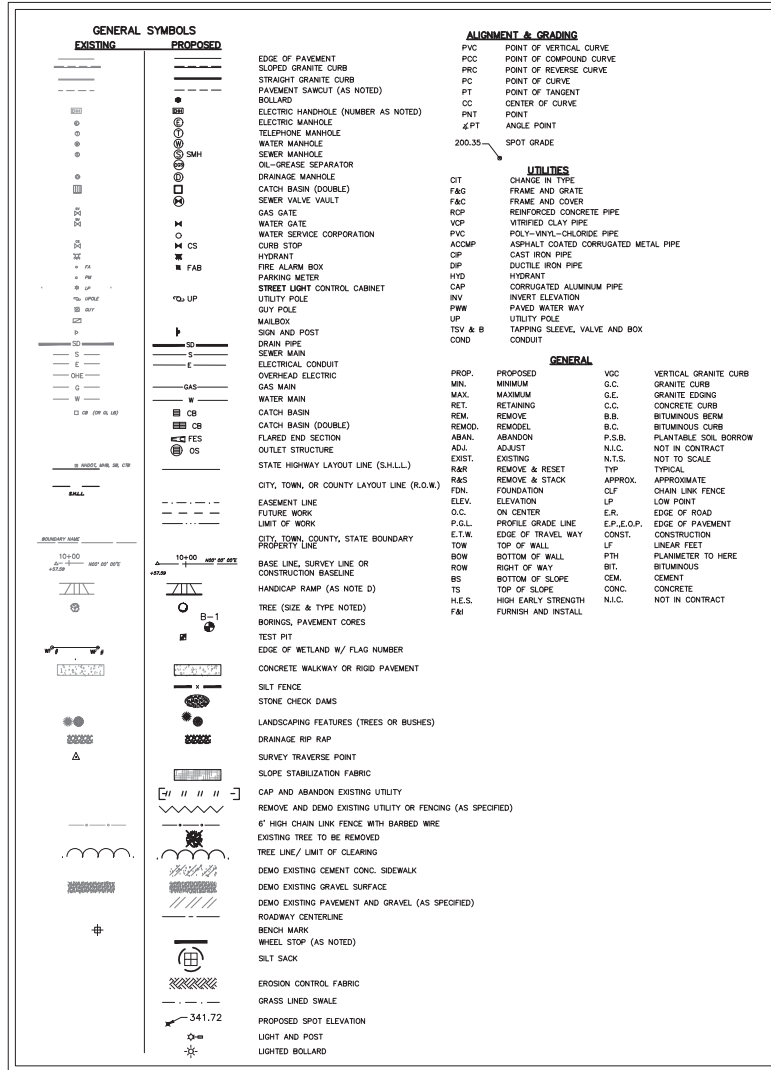
1. THE EXISTING CONDITIONS SITE PLAN THAT SERVES AS THE BASIS FOR THE READINESS CENTER DRAWINGS ARE THE RESULT OF A TOPOGRAPHIC SURVEY PERFORMED BY SAH LAND SERVICES, LLC IN DECEMBER 2018.
2. EXISTING UTILITIES FOR THE AAF'S BUILDING SITE ARE BASED ON AS BUILT DRAWINGS PREPARED BY SEA CONSULTANTS INC., FOR THE DEPARTMENT OF THE ARMY AND AIR FORCE, DATED MAY 17, 2002.
3. SOIL BORING SHOWN ON THE PLANS WERE PERFORMED BY SEABOARD DRILLING ON DECEMBER 21, 2018 AND WERE OBSERVED BY STANTEC AND ARE THE BASIS FOR THE GEOTECHNICAL REPORT IN THE APPENDIX OF THE SPECIFICATIONS.
4. WETLANDS WERE INVESTIGATED BY THOMAS SOKOLOSKI, NH CERTIFIED WETLANDS SCIENTIST#127 IN DECEMBER OF 2018. NO JURISDICTIONAL WETLAND WERE IDENTIFIED WITHIN OR IMMEDIATELY ADJACENT TO THE PROPOSED DEVELOPMENT SITE OR ACCESS CORRIDOR. WETLANDS WERE INVESTIGATED ACCORDING TO THE METHODOLOGY PRESENTED IN THE CORPS OF ENGINEERS WETLANDS DELINEATION (TECHNICAL REPORT Y-87-1), DATED AND THE REGIONAL SUPPLEMENT TO THE CORPS OF ENGINEERS WETLAND DELINEATION MANUAL, NORTH-CENTRAL AND NORTHEAST REGION, VERSION 2.0, JANUARY 2012, US ARMY CORPS OF ENGINEERS.

UTILITY SERVICES

1. COORDINATE ALL WORK AND INSPECTIONS REQUIRED BY THE UTILITY COMPANIES FOR CONSTRUCTION OF UTILITY SERVICES. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH COMPANY REQUIREMENTS, STANDARDS AND SPECIFICATIONS.
2. ELECTRIC SERVICE SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF EVERSOURCE.
3. ALL WATER SYSTEM WORK SHALL BE COORDINATED WITH THE CONCORD DPW WATER WORKS.
4. ALL SEWER SYSTEM WORK SHALL BE COORDINATED WITH THE CONCORD DPW SEWER DEPARTMENT.
5. TELEPHONE AND OTHER COMMUNICATIONS SERVICES SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPLICABLE STANDARDS FOR EACH UTILITY COMPANY.
6. FOR ELECTRIC, TELEPHONE AND OTHER COMMUNICATIONS, SEE ELECTRICAL SITE PLAN, DETAILS AND NOTES.
7. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING AND PAYING FOR THE SUPPORT OF ALL EXISTING UTILITY POLES, MANMADE FEATURES, FENCING, ECT.

MISCELLANEOUS

1. COORDINATE SITE AND UTILITY DETAILS WITH THE CITY OF CONCORD DEPARTMENT OF PUBLIC WORKS TYPICAL CONSTRUCTION DETAILS PRIOR TO CONSTRUCTION.



RESTORATION NOTES

LOADING

LOAM IS REQUIRED ON ANY AREA WHERE IT IS NECESSARY TO ESTABLISH A VEGETATIVE COVER. IF THERE IS INSUFFICIENT NATURALLY OCCURRING LOAM, ADDITIONAL LOAM SHALL BE PROVIDED TO A MINIMUM FINISHED DEPTH OF SIX INCHES.

1. LOAM SHALL BE USED FOR RESTORATION AND GRASS ESTABLISHMENT.
2. THE MATERIAL SHALL BE FRIABLE AND FREE OF TREE ROOTS, WEEDS, STONES (GREATER THAN ONE INCH) AND ANY OTHER DEBRIS. SOIL WHICH HAS BEEN TREATED WITH HERBICIDE IS UNACCEPTABLE.
3. THE MATERIAL SHALL BE TAKEN FROM THE NATURAL SURFACE LAYERS ("A" HORIZON) OF SOILS CAPABLE OF PRODUCING GOOD YIELDS OF CULTIVATED CROPS OR STRAW.
4. AREAS WHERE LOAM HAS BEEN REMOVED SHALL BE PROTECTED AGAINST EROSION.
5. LOAM SHALL NOT BE STOCKPILED WHERE IT WILL INTERFERE WITH ANY DRAINAGE COURSE OR WITH TWENTY-FIVE FEET OF A WETLAND RESOURCE AREA.
6. LOAM SHALL NOT BE COLLECTED OR SPREAD WHILE IT IS WET.
7. SUBSURFACES SHALL BE SCARIFIED OR OTHERWISE TILLED TO FACILITATE BONDING OF SOIL LAYERS.
8. LOAM SHALL BE UNIFORMLY SPREAD TO PROVIDE A MINIMUM FINISHED DEPTH OF SIX (6) INCHES AFTER SETTLEMENT FOR ALL AREAS EXCEPT DETENTION BASINS WHICH SHALL BE EIGHT (8) INCHES.

SEEDING

SITE PREPARATIONS

1. SHAPE AND SMOOTH THE SURFACE TO BE SPECIFIED ON DESIGN.
2. LIME ACCORDING TO SOIL TEST TO PH 6.5 OR IN THE ABSENCE OF A SOIL TEST, APPLY LIME AT THE RATE OF 2-3 TONS OF GROUND LIMESTONE PER ACRE (100-150 LBS. PER 1000 S.F.)
3. FERTILIZE ACCORDING TO SOIL TEST OR AT A RATE OF 1000 LBS. PER ACRE OF 5-10-10 FERTILIZER.
4. WORK UP A SEEDBED 2" TO 3" DEEP, THOROUGHLY INCORPORATING THE LIME AND FERTILIZER INTO THE SOIL.
5. SEED DURING THE PERIOD AUGUST 10 TO SEPTEMBER 15, OR IN THE SPRING BY MAY 20 FOR PERMANENT COVER. SEEDING AT OTHER TIMES SHALL BE CONSIDERED A TEMPORARY COVER.
6. APPLY SEED UNIFORMLY ACCORDING TO DIRECTIONS BY HYDRAULIC APPLICATION (HYDROSEEDING).
7. THE SEEDING AREA IS TO BE PROTECTED FROM EQUIPMENT, TRAMPLING AND OTHER DESTRUCTIVE ACTIVITY.
8. SEED MIX: PROVIDE A CARE-FREE, FINE FESCUE BLEND, OLDS SEED SOLUTIONS, "NO-MOW" LAWN MIX, AS FOLLOWS:
A. CREEPING RED FESCUE
B. HARD FESCUE
C. RED FESCUE
D. SHEEP FESCUE
E. CHEWING FESCUE
F. SEED APPLICATION SHALL BE 220 LBS/ACRE

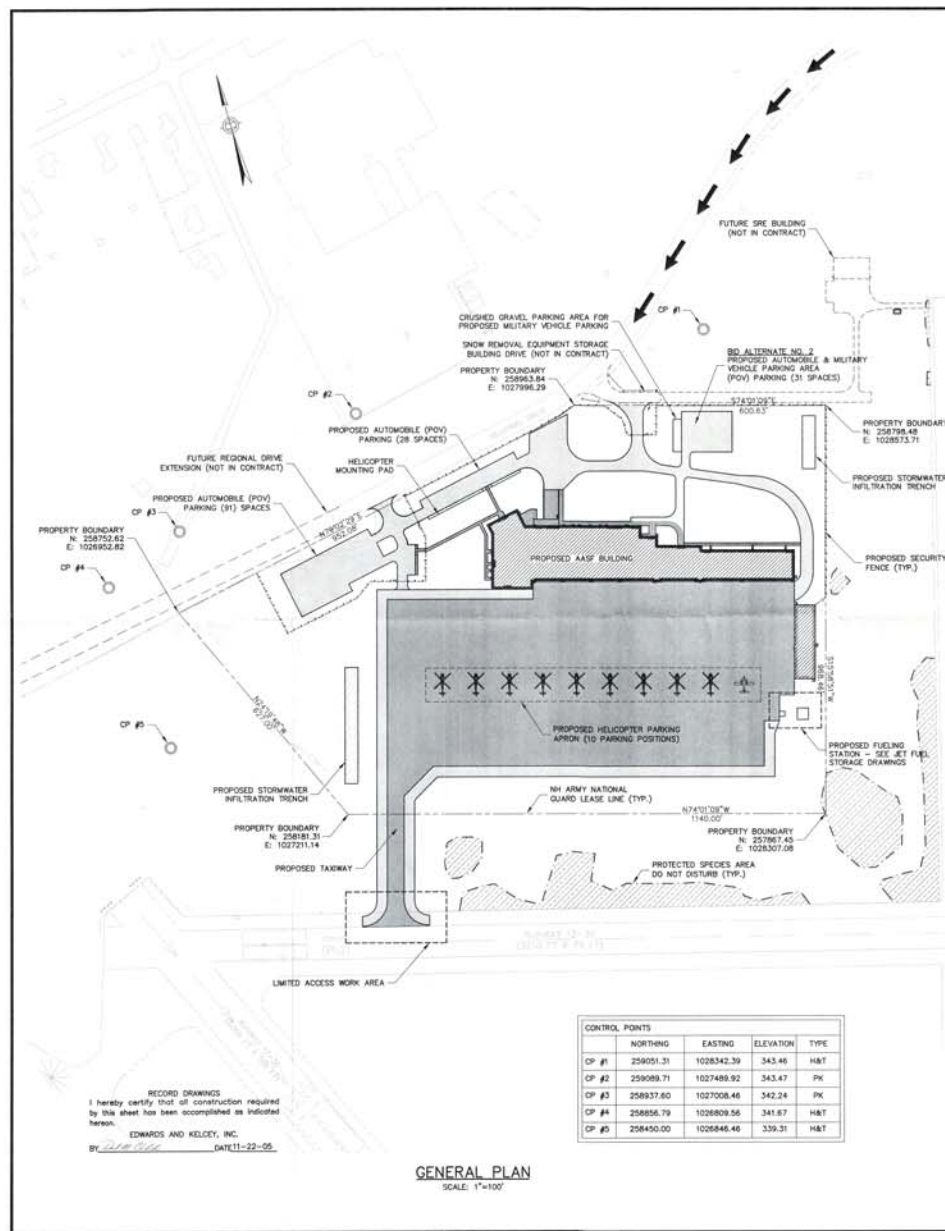
MULCHING

ON SOILS SUBJECT TO EROSION SUCH AS SLOPES AND ON SEEDING AREAS OF TEMPORARY COVER WHERE MOISTURE CONSERVATION IS NECESSARY FOR SEED GERMINATION, MULCH MATERIAL WILL BE USED.

1. WHERE FEASIBLE, COMPLETE EARTHWORK AND CONSTRUCTION ACCORDING TO PLANS.
 2. ALL ESSENTIAL GRADING NEEDED TO PREVENT EROSION AND SILTATION IS TO BE COMPLETED PRIOR TO APPLICATION OF MULCH MATERIAL.
- | APPLICATION RATE | | | |
|-----------------------------|---------------|----------|--|
| MULCH MATERIAL | PER 1000 S.F. | PER ACRE | REMARKS |
| STRAW OR STRAW | 70-90 LBS. | 1.5 TONS | LIGHTLY COVER
75%-90% OF SURFACE SUBJECT TO BLOWING AND SLIPPING UNLESS ANCHORED. |
| JUTE OR WOOD EXCLOSURE MATS | | | APPLY ACCORDING TO MANUFACTURER'S DIRECTIONS. USE IN AREA OF HIGH FLOWS. |
3. HYDROMULCH WITH 1 1/2 TONS OF STRAW PER ACRE ON ALL DISTURBED AREAS IMMEDIATELY AFTER SEEDING.
 4. MULCHES SUSCEPTIBLE TO BLOWING AND THOSE ON STEEP SLOPES WILL REQUIRE ANCHORING.

	BUILDING AREA (SF)	REQUIRED PARKING SPACES (1 PER 400 SF)	ACTUAL PARKING SPACES	REQUIRED ACCESSIBLE PARKING SPACES (1 PER 6 SPACES)	ACTUAL ACCESSIBLE SPACES
EXISTING	82000	205	151	25	5
PROPOSED	96700	242	257	43	6

	PAVEMENT AREA (SF)	REQUIRED NUMBER OF TREES ON SITE (1 TREE PER 2,000 SF)	ACTUAL NUMBER OF TREES
EXISTING	107,700	54	293
PROPOSED	142,900	72	293



NOTES:

UNRESTRICTED WORK AREA

ALL CONSTRUCTION OPERATIONS WERE CARRIED ON WITHIN 75 FEET (22.8 M) FROM THE EDGE OF ANY TAXWAY OR WITHIN 250 FEET (76.2 M) OF THE CENTERLINE OF ANY ACTIVE RUNWAY OR WITHIN THE LIMITS OF ACTIVE RUNWAY APPROACH ZONES UNLESS PRIOR APPROVAL WAS OBTAINED. WHEN PERMISSION WAS GRANTED TO WORK INSIDE THESE LIMITS, NO EQUIPMENT WAS LEFT WITHIN THE LIMITS WHEN NOT ACTUALLY WORKING. DURING LUNCH HOUR BREAKS IN THE DAILY WORK SCHEDULE, AND THE DAYS WHEN WORK WAS NOT PERMITTED OR WAS NOT PROCEEDING, THE EQUIPMENT WAS LOCATED OUTSIDE OF THE RESTRICTED AREA. ROADWAYS WERE MAINTAINED BY THE CONTRACTOR AND CONSTRUCTION OPERATIONS, INCLUDING AN OPEN FLAME SUCH AS WELDING OR BURNING, WERE CARRIED ON NEAR AN AIRPORT. EQUIPMENT WAS TO BE STORED IN THE CONTRACTOR'S STAGING AREAS DURING NIGHTS AND WEEKENDS WHEN NO WORK WAS SCHEDULED.

HAUL ROUTES

HAUL ROUTES WERE USED FOR HAUL ROUTES IF BECAME THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN THE PROPER PERMITS FOR THIS FUNCTION AND TO OBEY ALL RULES AND REGULATIONS PERTAINING TO THE PUBLIC HIGHWAYS. HAUL ROUTES ON THE AIRPORT WERE AS SHOWN ON THE CONTRACT DRAWINGS. THE CONTRACTOR WAS TO STAKE OR OTHERWISE CLEARLY DELINEATE THE HAUL ROUTES. THE CONTRACTOR VEHICLES WERE NOT ALLOWED ACCESS TO PORTIONS OF THE AIRPORT OTHER THAN THE WORK AND STAGING AREAS. ALL PAVED HAUL ROADS OR ACCESS ROADS WERE KEPT CLEAN AT ALL TIMES TO PREVENT THE ACCUMULATION OF DIRT AND GRASS AND THE GENERATION OF DUST BY IMPROPER WASHING OF OTHER METHODS DIRECTED BY THE CONTRACTING OFFICER. UNPAVED HAUL ROADS WERE MAINTAINED BY GRADING AND FILLING WHEN DIRECTED BY THE CONTRACTING OFFICER AND DUST WAS CONTROLLED AT ALL TIMES. ALL PAVED HAUL ROADS DISTURBED WERE RESTORED AND DUST CONTROL ON HAUL ROADS WERE MAINTAINED. ALL NONPAVED AREAS ON THE AIRPORT DISTURBED BY THE CONTRACTOR'S OPERATIONS WERE CONSIDERED COMPLETE. ALL MAINTENANCE, RESTORATION, AND DUST CONTROL ON HAUL ROADS WAS AT THE CONTRACTOR'S EXPENSE. ALL NONPAVED AREAS ON THE AIRPORT DISTURBED BY THE CONTRACTOR'S OPERATIONS WERE RESTORED OR OTHERWISE LEAVING TO A DEPTH OF NOT LESS THAN 3 INCHES. CLAYS WERE BROKEN AND THE TOP 3 INCHES OF SOIL WAS MOVED INTO A SATISFACTORY DEEPNESS BY FLOODING, OR BY USE OF CULTIVATORS, ROLLERS, GRADERS, HARROWS, OR OTHER APPROPRIATE MEANS. THIS AREA WAS THEN GRADED, FINE GRADED, SEDED, FERTILIZED AND MULCHED, AT NO ADDITIONAL COST TO THE OWNER.

STORAGE AREA AND EQUIPMENT - 1800

THE CONTRACTOR WAS REQUIRED TO RETURN ALL EQUIPMENT TO THE APPROPRIATE CONTRACTOR'S STAGING AREA AT THE END OF THE WORK, EACH DAY UNLESS OTHERWISE APPROVED BY THE CONTRACTING OFFICER. ALL EQUIPMENT ROOMS WERE LOCATED AT THE CLOSE OF EACH DAY'S WORK. ALL EQUIPMENT WAS PARKED IN THE STAGING AREA AT THE CLOSE OF EACH DAY AND WHENEVER IT WAS NOT IN USE, THE CONTRACTOR (AND HIS SUBCONTRACTORS) WERE TO PROVIDE ALL NECESSARY TEMPORARY FENCING AND GATES TO PROTECT MATERIALS AND EQUIPMENT FROM THEFT. THE OWNER WAS NOT RESPONSIBLE FOR ANY UNAUTHORIZED EQUIPMENT OR MATERIALS STORAGE ON THE AIRPORT PROPERTY. ANY AREA OCCUPIED BY THE CONTRACTOR WAS MAINTAINED IN A CLEAN AND ORDERLY CONDITION SATISFACTORY TO THE CONTRACTING OFFICER. PARTICULAR ATTENTION WAS GIVEN TO THE ELIMINATION OF COMBUSTIBLE RUBBISH OR DEBRIS IN THE AREAS AND NONE WAS LEFT EXPOSED OVERNIGHT OR AT OTHER PERIODS OF TIME THE WORK WAS STOPPED.

CONTRACTOR STAGING AND STORAGE AREA

THE CONTRACTOR WAS ALLOWED TO PROVIDE HANDY WITH A STAGING AND STORAGE AREA PROVIDED IT REMAINED WITHIN THE LIMIT OF THE PROPOSED WORK. THROUGHOUT THE CONSTRUCTION PERIOD THE CONTRACTOR WAS TO ALSO MAKE PROVISIONS FOR CONTRACTOR EMPLOYEE PARKING WHICH WAS ALSO IN A DESIGNATED AREA APPROVED BY THE CONTRACTING OFFICER WITHIN THE LIMIT OF THE PROPOSED WORK.

OPEN TRENCHES OR EXCAVATIONS

THE CONTRACTOR WAS NOT PERMITTED TO LEAVE ANY TRENCHES OR OTHER EXCAVATIONS OPEN AT NIGHT, ON WEEKENDS OR AT OTHER TIMES WHEN THE CONTRACTOR WAS NOT ON THE SITE. IN ADDITION, NO AIRPORT CHANGES IN GRADE EXCEEDING 3 INCHES (76.2 M) IN 10 FEET WERE LEFT WITHIN THE AIRPORT OPERATIONS AREAS UNLESS THE SPECIFICATIONS WERE THE RUNWAYS, TAXWAYS, AND APRONS WERE IN USE. UNLESS THE EXCAVATIONS WERE COVERED WITH APPROVED STEEL PLATES, STEEL PLATES WERE TO BE CAPABLE OF BEARING THE HEAVIEST AIRCRAFT EXPECTED TO USE THE AIRPORT OVER THE AREA TO BE EXCAVATED. THE CONTRACTOR WAS TO KEEP THE LENGTH OF OPEN TRENCHES COVERED WITH STEEL PLATES TO A MINIMUM, BUT IN NO CASE, 300 FEET LENGTH EXCEEDED DISTANCE BETWEEN TWO ADJACENT STRUCTURES.

PRIOR TO THE CLOSE OF WORK EACH DAY, THE CONTRACTOR WAS TO ENSURE THAT THE WORK AREA WITHIN THE AIRPORT OPERATIONS AREA OF THE RUNWAYS, TAXWAYS AND APRONS WERE GRADED AWAY FROM THE PAVEMENTS AT A MINIMUM SLOPE OF 3% AND WERE LEFT IN SUCH CONDITION THAT IT WOULD DRAIN READILY AND EFFECTIVELY AND WOULD NOT FORM A HAZARD TO AIRCRAFT. NO PILES OF SOIL WERE LEFT UNPAVED, NO SHARP CHANGES IN GRADE WERE PERMITTED, AND THE SURFACE WAS THOROUGHLY COMPACTED.

AIRPORT OPERATION AND SAFETY REQUIREMENTS

NORMAL AIRPORT OPERATIONS WERE CONDUCTED ON THE AIRFIELD DURING CONSTRUCTION AND THE WORK WAS CARRIED ON IN SUCH A MANNER AS TO NOT INTERFERE WITH THE NECESSARY OPERATION OF THE AIRPORT. THE CONTRACTOR WAS TO TAKE ALL PRECAUTIONS NECESSARY TO ENSURE THE SAFETY OF OPERATING AIRCRAFT, AS WELL AS HELICOPTER OWN EQUIPMENT AND PERSONNEL.

EACH CONTRACTOR'S MOTORIZED VEHICLE OPERATING IN AN AIRPORT MOVEMENT AREA WAS EQUIPPED WITH AN AMBER FLASHING LIGHT SATISFACTORY TO THE CONTRACTING OFFICER, AND A 3-FOOT (0.9 M) SQUARE FLAG CONSISTING OF INTERNATIONAL ORANGE AND WHITE SQUARES NOT LESS THAN 1 FOOT (0.3 M) SQUARE DISPLAYED IN FULL VIEW AHEAD OF THE VEHICLE.

IN ADDITION, ALL CONTRACTOR'S VEHICLES WERE TO HAVE THE COMPANY CONTRACTOR PLANT VISIBLE ON BOTH SIDES OF THE VEHICLE IN ORDER TO IDENTIFY THE VEHICLE. THE CONTRACTOR WAS TO OBEY ALL INSTRUCTIONS AS TO THE OPERATION AND ROUTES TO BE TAKEN BY EQUIPMENT TRAVELING ON AIRPORT PROPERTY. ANY SIGNAL LIGHTS, SIGNALS, WARNING, TRAFFIC CONTROL, AND OTHER DEVICES WHICH MAY HAVE BEEN REQUIRED WERE PROVIDED AND MAINTAINED BY THE CONTRACTOR DURING THE COURSE OF THE WORK, SUBJECT TO THE APPROVAL OF THE CONTRACTING OFFICER. NO AIRPORT PAVEMENT OR NAVIGATION OR CURRENTLY IN SERVICE WAS LEFT OUT OF SERVICE OR DAMAGED UNLESS CLOSED TO ALL AIRPORT OPERATIONS. THE CONTRACTOR WAS TO CHECK ALL PAVEMENT AND TEMPORARY LIGHTING TO ENSURE ITS OPERATING CONDITION BEFORE LEAVING THE JOB EACH DAY.

THE CONTRACTOR WAS TO STAKE AND PERMANENTLY MARK ON THE GROUND WITH A READILY RECOGNIZABLE MARKING (FOOTBALL OR SIMILAR MATERIAL) THE RESTRICTION LINES PARALLEL TO THE TAXWAYS AND RUNWAYS ADJACENT TO THE WORK AND THE APPROACH ZONE LIMITS SO THAT WORKMEN COULD READILY RECOGNIZE THE LIMITATIONS. THE COST OF LAYING OUT AND MARKING SUCH RESTRICTION LINES WAS NOT PAID FOR SEPARATELY BUT WAS CONSIDERED INCIDENTAL TO THE PROJECT.

MAINTENANCE OF THE CONSTRUCTION SITE

THE CONTRACTOR WAS TO KEEP THE CONSTRUCTION SITE FREE OF PAPER, RUBBER, AND OTHER DEBRIS WHICH COULD HAVE BLOWN DOWN THE RUNWAYS AND TAXWAYS. ALL AIRPORT PAVEMENTS WERE KEPT CLEAR AND CLEAN AT ALL TIMES. ALL ROCKS, MUD, AND OTHER DEBRIS CARRIED ONTO THE AIRPORT PAVEMENT BY THE CONTRACTOR'S EQUIPMENT WAS REPORTED TO THE CONTRACTING OFFICER OR THE AIRPORT DIRECTOR'S OFFICE.

THE AIRPORT DIRECTOR WOULD THEN CLOSE THE AFFECTED AREA TO AIR TRAFFIC AND THE CONTRACTOR IMMEDIATELY SHUT THE AREA TO THE SATISFACTION OF THE AIRPORT DIRECTOR. THE CONTRACTOR MAINTAINED AT THE CONSTRUCTION SITE THE EQUIPMENT FOR THE APPLICATION OF WATER TO CONTROL DUST WHEN THE CONSTRUCTION SITE AND ON HAUL ROADS. THE EQUIPMENT WAS EQUIPPED WITH A SHUT-OFF CONTROL VALVE WHICH COULD BE OPERATED FROM THE CAB BY THE CONTRACTOR'S OPERATOR. THE CONTRACTOR WAS TO APPLY WATER FOR DUST CONTROL AS NECESSARY TO PREVENT DUST FROM THE CONSTRUCTION SITE AND/OR HAUL ROADS FROM BEING A HAZARD TO AIRCRAFT AND FROM BEING A NUISANCE TO THE PUBLIC AND AS DIRECTED BY THE CONTRACTING OFFICER.

THE CONTRACTOR WAS TO MAINTAIN AT THE JOB SITE AT ALL TIMES WHILE THE CONSTRUCTION UNDER THIS CONTRACT WAS IN PROGRESS, A SELF-PROPELLED, SELF-CONTAINED WORKER WITH NOT LESS THAN A 10-FOOT BEAM WITH A 4" CURB AND 100 CAPACITY WORKER. THE BEAMER OPERATED AS NECESSARY TO KEEP ACTIVE AIRPORT PAVEMENTS, ACCESS ROADS AND THE WORK AREAS CLEAN. AT THE CLOSE OF EACH DAY'S WORK, ALL ACTIVE AIRPORT PAVEMENTS AND AIRPORT PAVED ROADS USED OR DIRECTED BY THE CONTRACTOR WERE TO AGAIN BE SWEPT.

THE CONTRACTOR WAS ALSO RESPONSIBLE FOR SUPPLYING ANY OTHER EQUIPMENT AS MAY BE NECESSARY TO CLEAN ALL AREAS THAT ARE CONTAMINATED AS A RESULT OF HELICOPTER OPERATIONS TO THE COMPLETE SATISFACTION OF THE CONTRACTING OFFICER AND THE AIRPORT DIRECTOR.

TRUCKS LOADED IN THE CONSTRUCTION AREA WERE TO HAVE LOADS TRIMMED AND COVERED AS NECESSARY TO ENSURE THAT NO PARTICLES, STONES, OR DEBRIS WOULD FALL OFF AND THAT NO LEGAL LOAD LIMITS ARE EXCEEDED.

THE CONTRACTOR WAS TO BE PARTICULARLY CAREFUL NOT TO TRACK FOREIGN MATERIAL ONTO PAVEMENTS OUTSIDE OF AND WITHIN THE AIRPORT (E.G., TACK-COAT). THE CONTRACTOR WAS RESPONSIBLE FOR REMOVING FOREIGN MATERIALS FROM VEHICLE TIRES PRIOR TO THE VEHICLE LEAVING ITS WORK AREA.

UNRESTRICTED WORK AREA

ALL WORK WITHIN THIS AREA WAS CLOSELY COORDINATED WITH THE NEW HAMPSHIRE ARMY NATIONAL GUARD, THE CITY OF CONCORD, AND THE NEW HAMPSHIRE DIVISION OF AERONAUTICS. THE CONTRACTOR WAS TO COORDINATE HIS OPERATIONS WITH THOSE OF OTHER CONTRACTORS. COOPERATION WAS REQUIRED IN THE ARRANGEMENT FOR THE STORAGE OF MATERIALS AND IN THE DETAILED EXECUTION OF THE WORK. THE CONTRACTOR, INCLUDING HIS SUBCONTRACTORS, WAS TO KEEP INFORMED OF THE PROGRESS AND THE DETAIL WORK OF OTHER CONTRACTORS AND NOTIFIED THE OTHER IMMEDIATELY OF LACK OF PROGRESS OR DEFECTIVE WORKMANSHIP ON THE PART OF OTHER CONTRACTORS.

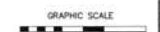
THE CONTRACTOR WAS TO ARRANGE HELICOPTER WORK AND PLACE AND DISPOSE OF THE MATERIALS BEING USED SO AS NOT TO INTERFERE WITH THE OPERATIONS OF THE OTHER CONTRACTORS WITHIN THE LIMITS OF THE SAME PROJECT. HE COULD WORKER WITH THAT OF THE OTHERS IN AN ACCEPTABLE MANNER AND WAS TO FURNISH IT IN PROPER SEQUENCE TO THAT OF THE OTHERS.

NO ALTERNATE #2

THE PROPOSED SECURE AUTOMOBILE PARKING AREA WAS TO BE COMPLETED AS PART OF NO. 2. ALL OTHER WORK ASSOCIATED WITH THE CONSTRUCTION OF THIS AREA WAS INCLUDED WITH THE SAME NO. 2.

LEGEND

- PROPOSED BUILDING
- PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT
- PROPOSED BITUMINOUS CONCRETE PAVEMENT
- PROTECTED SPECIES AREA - DO NOT DISTURB
- PROPOSED SECURITY FENCE
- NH ARMY NATIONAL GUARD LEASE LINE
- CONTRACTOR'S HAUL ROUTE
- CONTROL POINT



SPECIAL ENVIRONMENTAL NOTICE

THE PROTECTED SPECIES AREA DESIGNATED ON THE PLANS IS HOME TO THE WILD BLUE LUPINE PLANT WHICH PROVIDES A HABITAT FOR THE RARE BUTTERFLY. THIS BUTTERFLY IS DESIGNATED AS AN ENDANGERED SPECIES BY THE U.S. FISH AND WILDLIFE SERVICE. SPECIAL CONSIDERATION WAS TO BE TAKEN BY THE CONTRACTOR TO PROTECT THIS BUTTERFLY AND ITS HABITAT.

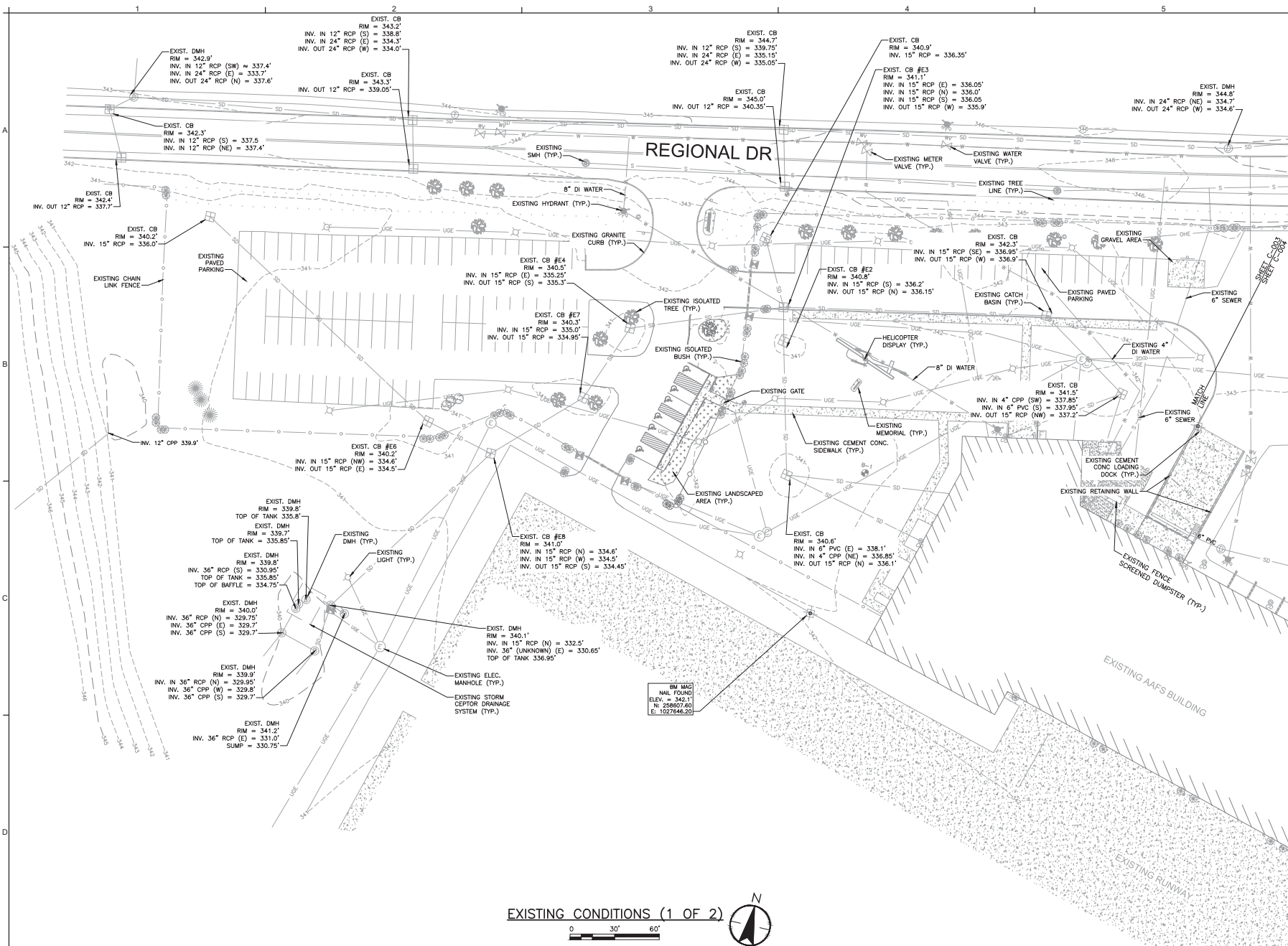
REV	NO	DATE	DESCRIPTION	MADE BY	CHECKED BY	APPROVED BY
1	11/05		RECORD DRAWINGS	EST	BLN	DMC

ARMY AVIATION SUPPORT FACILITY			
PROJECT NO. 97828-R/230028			
ARMY - NEW HAMPSHIRE NATIONAL GUARD			
CONCORD AIRPORT			
Drawing Title:		GENERAL PLAN	
Designed By:	BLN	Drawn By:	LAG
Checked By:	DMC	Date:	OCT 25, 2002
In Charge:	CAT	Scale:	1"=100'
		Sheet No.:	C-2

RECEIVED

JUL 16 2002

Planning Division
Concord, NH



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COOVER CLARK & ASSOCIATES, INC.
DESIGN: COLORADO
CONTRACT: 0000000000

VERIFY SCALE:
BAR IS ONE INCH ON
ORIGINAL DRAWING
0" = 100' 1" = 100'
IF NOT ONE INCH ON THIS SHEET,
ADJUST SCALES ACCORDINGLY

B. RUOFF	NHARRING
DESIGNED BY	DESIGN AGENCY
S&H	D. ANDRUS
BASE SHEET BY	FIRE CHIEF
E. BELISLE	N/A
DRAWN BY	SD-ENVR. ENGR.
R. LABRANCHE	J.D. TATEM
CHECKED BY	SAFETY
CCA	N/A
APPROVED BY	COMMUNICATION

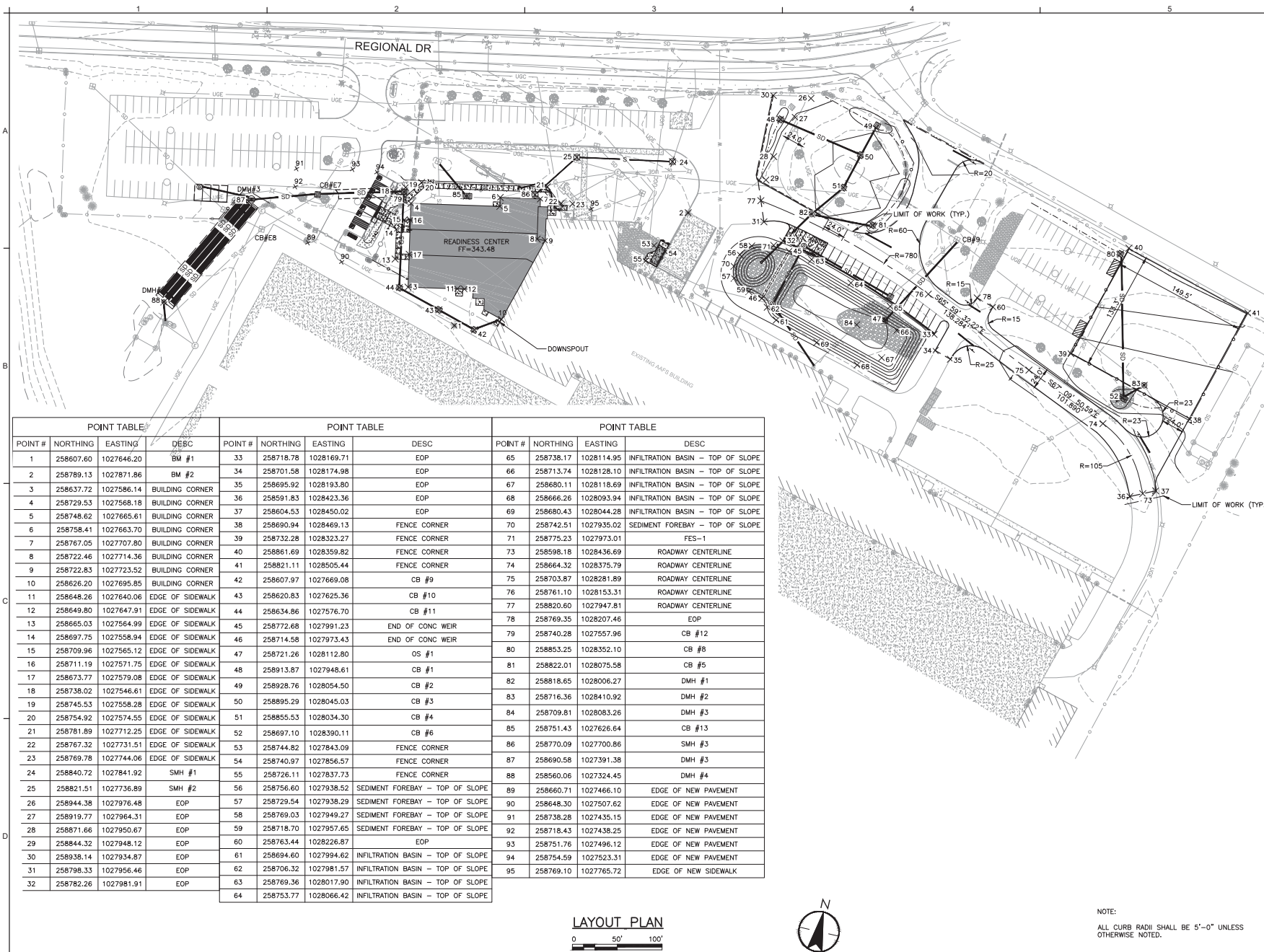
195113343

PROJECT NUMBER:
81018R
COOVER-CLARK JOB NUMBER:
201803

PROJECT TITLE:
**CONCORD AVIATION
READINESS CENTER**

DRAWING TITLE:
**EXISTING CONDITIONS
(1 OF 2)**

DATE JULY, 2020	PAGE NUMBER C003
REV.	



POINT TABLE				POINT TABLE				POINT TABLE			
POINT #	NORTHING	EASTING	DESC	POINT #	NORTHING	EASTING	DESC	POINT #	NORTHING	EASTING	DESC
1	258607.60	1027646.20	BM #1	33	258718.78	1028169.71	EOP	65	258738.17	1028114.95	INFILTRATION BASIN - TOP OF SLOPE
2	258789.13	1027871.86	BM #2	34	258701.58	1028174.98	EOP	66	258713.74	1028128.10	INFILTRATION BASIN - TOP OF SLOPE
3	258637.72	1027586.14	BUILDING CORNER	35	258695.92	1028193.80	EOP	67	258680.11	1028118.69	INFILTRATION BASIN - TOP OF SLOPE
4	258729.53	1027568.18	BUILDING CORNER	36	258591.83	1028423.36	EOP	68	258666.26	1028093.94	INFILTRATION BASIN - TOP OF SLOPE
5	258748.62	1027665.61	BUILDING CORNER	37	258604.53	1028450.02	EOP	69	258680.43	1028044.28	INFILTRATION BASIN - TOP OF SLOPE
6	258758.41	1027663.70	BUILDING CORNER	38	258690.94	1028469.13	FENCE CORNER	70	258742.51	1027935.02	SEDIMENT FOREBAY - TOP OF SLOPE
7	258767.05	1027707.80	BUILDING CORNER	39	258732.28	1028323.27	FENCE CORNER	71	258775.23	1027973.01	FES-1
8	258722.46	1027714.36	BUILDING CORNER	40	258861.69	1028359.82	FENCE CORNER	73	258598.18	1028436.69	ROADWAY CENTERLINE
9	258722.83	1027723.52	BUILDING CORNER	41	258821.11	1028505.44	FENCE CORNER	74	258664.32	1028375.79	ROADWAY CENTERLINE
10	258626.20	1027695.85	BUILDING CORNER	42	258607.97	1027669.08	CB #9	75	258703.87	1028281.89	ROADWAY CENTERLINE
11	258648.26	1027640.06	EDGE OF SIDEWALK	43	258620.83	1027625.36	CB #10	76	258761.10	1028153.31	ROADWAY CENTERLINE
12	258649.80	1027647.91	EDGE OF SIDEWALK	44	258634.86	1027576.70	CB #11	77	258820.60	1027947.81	ROADWAY CENTERLINE
13	258665.03	1027564.99	EDGE OF SIDEWALK	45	258772.68	1027991.23	END OF CONC WEIR	78	258769.35	1028207.46	EOP
14	258697.75	1027558.94	EDGE OF SIDEWALK	46	258714.58	1027973.43	END OF CONC WEIR	79	258740.28	1027557.96	CB #12
15	258709.96	1027565.12	EDGE OF SIDEWALK	47	258721.26	1028112.80	OS #1	80	258853.25	1028352.10	CB #8
16	258711.19	1027571.75	EDGE OF SIDEWALK	48	258913.87	1027948.61	CB #1	81	258822.01	1028075.58	CB #5
17	258673.77	1027579.08	EDGE OF SIDEWALK	49	258928.76	1028054.50	CB #2	82	258818.65	1028006.27	DMH #1
18	258738.02	1027546.61	EDGE OF SIDEWALK	50	258895.29	1028045.03	CB #3	83	258716.36	1028410.92	DMH #2
19	258745.53	1027558.28	EDGE OF SIDEWALK	51	258855.53	1028034.30	CB #4	84	258709.81	1028083.26	DMH #3
20	258754.92	1027574.55	EDGE OF SIDEWALK	52	258697.10	1028390.11	CB #6	85	258751.43	1027826.64	CB #13
21	258781.89	1027712.25	EDGE OF SIDEWALK	53	258744.82	1027843.09	FENCE CORNER	86	258770.09	1027700.86	SMH #3
22	258767.32	1027731.51	EDGE OF SIDEWALK	54	258740.97	1027856.57	FENCE CORNER	87	258690.58	1027391.38	DMH #3
23	258769.78	1027744.06	EDGE OF SIDEWALK	55	258726.11	1027837.73	FENCE CORNER	88	258560.06	1027324.45	DMH #4
24	258840.72	1027841.92	SMH #1	56	258756.60	1027938.52	SEDIMENT FOREBAY - TOP OF SLOPE	89	258660.71	1027466.10	EDGE OF NEW PAVEMENT
25	258821.51	1027736.89	SMH #2	57	258729.54	1027938.29	SEDIMENT FOREBAY - TOP OF SLOPE	90	258648.30	1027507.62	EDGE OF NEW PAVEMENT
26	258944.38	1027976.48	EOP	58	258769.03	1027949.27	SEDIMENT FOREBAY - TOP OF SLOPE	91	258738.28	1027435.15	EDGE OF NEW PAVEMENT
27	258919.77	1027964.31	EOP	59	258718.70	1027957.65	SEDIMENT FOREBAY - TOP OF SLOPE	92	258718.43	1027438.25	EDGE OF NEW PAVEMENT
28	258871.66	1027950.67	EOP	60	258763.44	1028226.87	EOP	93	258751.76	1027496.12	EDGE OF NEW PAVEMENT
29	258844.32	1027948.12	EOP	61	258694.60	1027994.62	INFILTRATION BASIN - TOP OF SLOPE	94	258754.59	1027523.31	EDGE OF NEW PAVEMENT
30	258938.14	1027934.87	EOP	62	258706.32	1027981.57	INFILTRATION BASIN - TOP OF SLOPE	95	258769.10	1027765.72	EDGE OF NEW SIDEWALK
31	258798.33	1027956.46	EOP	63	258769.36	1028017.90	INFILTRATION BASIN - TOP OF SLOPE				
32	258782.26	1027981.91	EOP	64	258753.77	1028066.42	INFILTRATION BASIN - TOP OF SLOPE				

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STANTEC

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DESIGNED BY	USING AGENCY
S&H	D. ANDRUS
BASE SHEET BY	FIRE CHIEF
E. BELISLE	N/A
DRAWN BY	SD-ENVR. ENGR.
R. LABRANCHE	J.D. TATEM
CHECKED BY	SAFETY
CCA	N/A
APPROVED BY	COMMUNICATION

195113343

PROJECT NUMBER:
81018R

COOVER-CLARK JOB NUMBER:
201803

PROJECT TITLE:

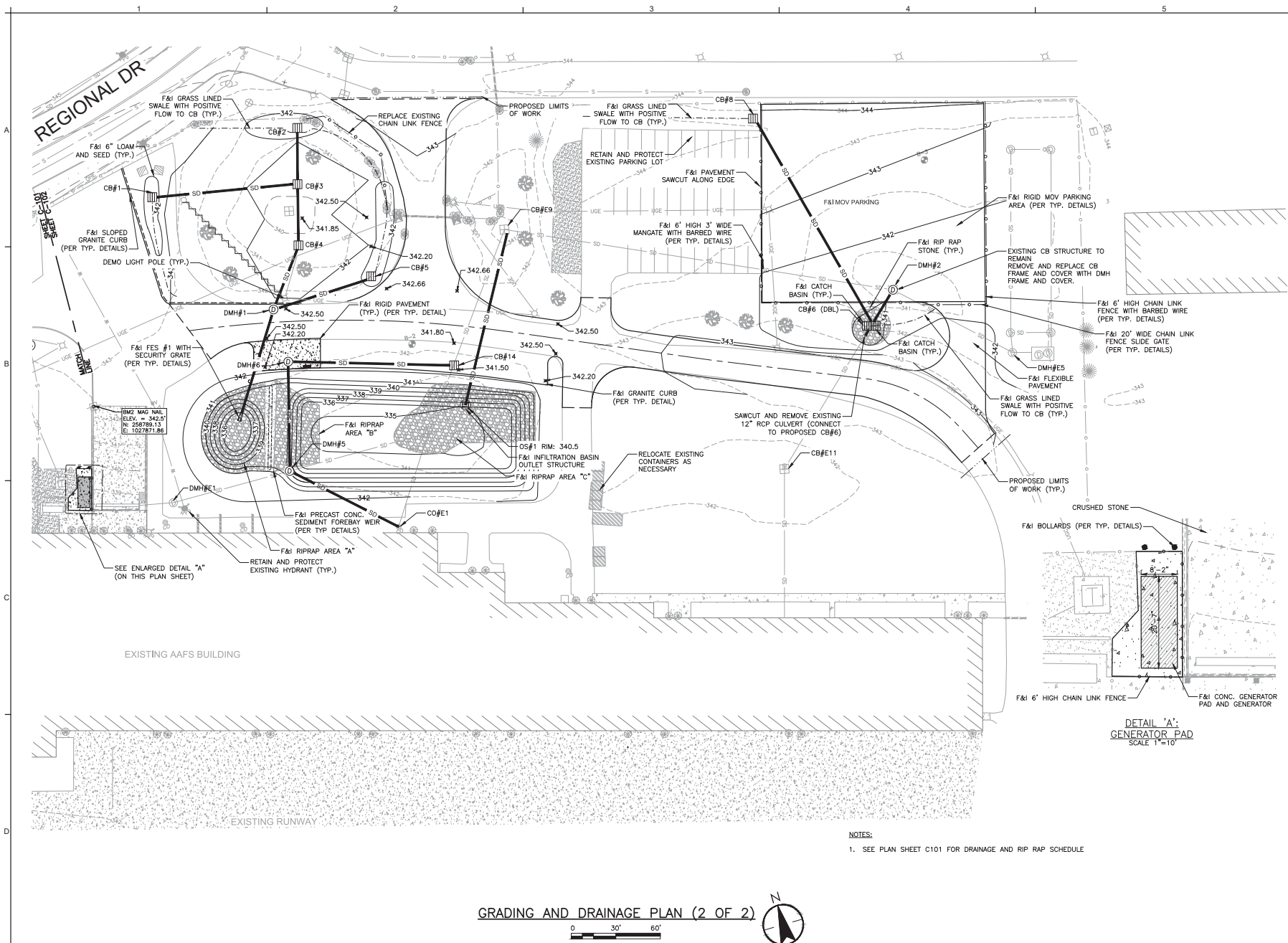
CONCORD AVIATION
READINESS CENTER

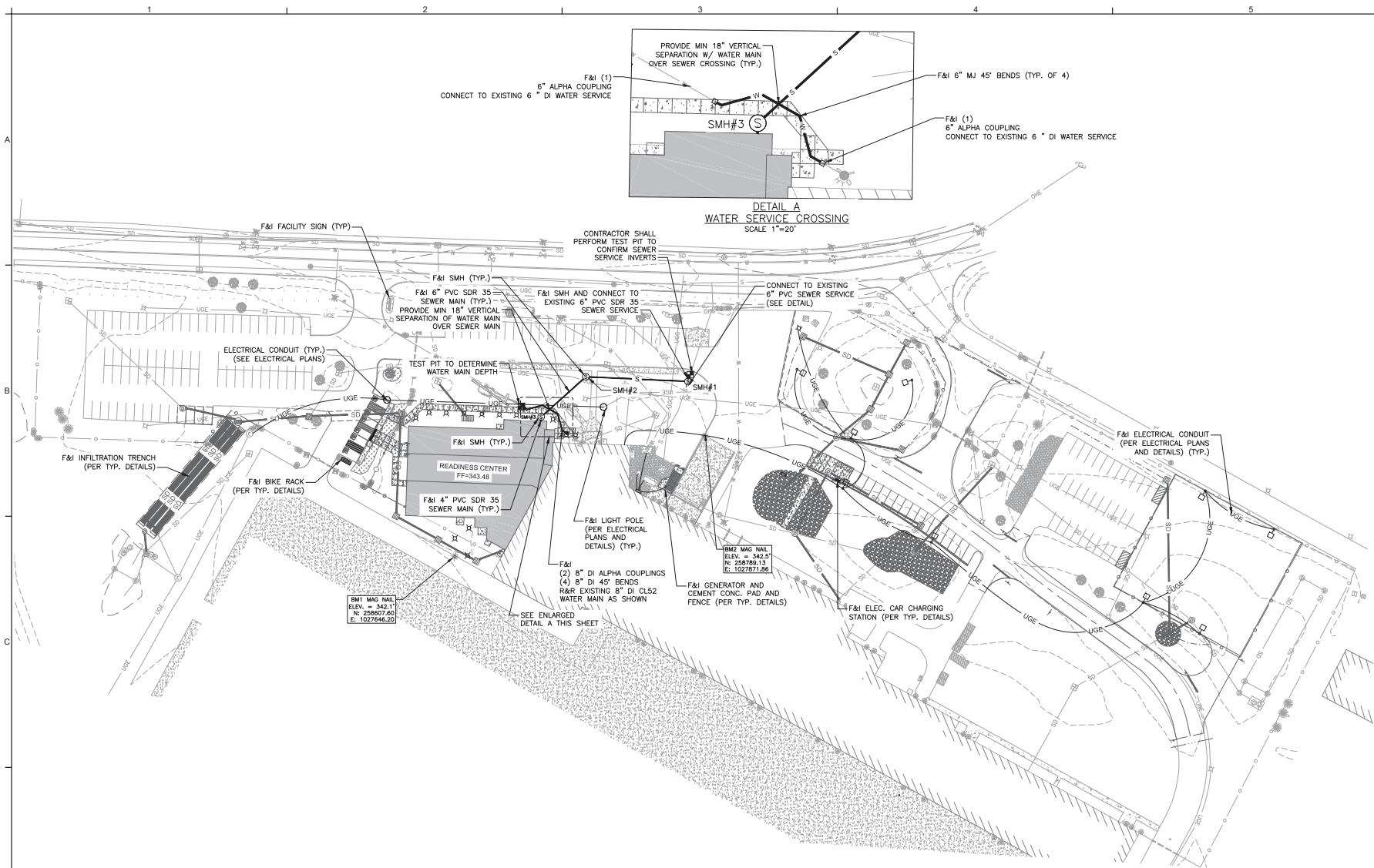
DRAWING TITLE:

LAYOUT PLAN

DATE JULY, 2020	PAGE NUMBER C100
REV.	

PLOTTED: 7/16/2020



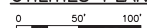


DETAIL A
WATER SERVICE CROSSING
SCALE 1"=20'

SANITARY SEWER SYSTEM SCHEDULE

BUILDING LATERAL	SMH#2
4" PVC INV OUT=337.48	RIM=342.25
5 LF 4" PVC SDR 35 0.06 FT/FT	6" PVC INV IN=334.00
SMH #3 (INTERNAL DROP INLET)	6" PVC INV OUT=333.90
RIM=342.80	100 LF 6" PVC SDR35, S=0.015 FT/FT
4" PVC INV IN=337.18	SMH#1
6" PVC INV OUT=334.55	RIM=342.25
55 LF 6" PVC SDR35, S=0.01 FT/FT	6" PVC INV IN=332.39
	6" PVC INV IN=332.39± (CONNECT TO EXISTING) CONFIRM BY TEST PIT
	6" PVC INV OUT=332.34± (CONNECT TO EXISTING) CONFIRM BY TEST PIT

UTILITIES PLAN



SUGGESTED SEQUENCE OF SANITARY SEWER WORK:

1. PRIOR TO FABRICATING MANHOLES OR INSTALLATION OF THE SEWER SERVICE LINE, THE CONTRACTOR SHALL PERFORM A TEST PIT TO CONFIRM EXISTING PIPE INVERT ELEVATION, PIPE MATERIAL, AND PIPE SIZE AT THE PROPOSED SEWER LATERAL TIE IN LOCATION WITH SMH#3. THIS INFORMATION SHALL BE PROVIDED TO THE CONTRACT ADMINISTRATOR.
2. THE CONTRACTOR SHALL SUBMIT THE TEST PIT INFORMATION TO THE ENGINEER TO COORDINATE THE SMH STRUCTURE INVERTS PRIOR TO FABRICATION.
3. THE ENGINEER SHALL CONFIRM OR AMEND THE PROPOSED SMH INVERT ELEVATIONS, BASED ON THE RESULTS OF THE TEST PIT INFORMATION.

NOTES:

1. SEE PLAN SHEETS C101 AND C102 FOR GRADING AND DRAINAGE.
2. MINIMUM UTILITY SEPARATION:
A. WATER TO SEWER (GRAVITY AND FM) = 10'
B. SEPARATION OF ALL UTILITIES TO WATER OR SEWER = 5'
C. MIN. VERTICAL SEPARATION BETWEEN WATER AND SEWER IS 18".

B. R/LIOFF	N.HARRING
DESIGNED BY	USING AGENCY
SAH	D. ANDRUS
BASE SHEET BY	FIRE CHIEF
E. BELISLE	N/A
DRAWN BY	BO-ENVR. ENGR.
R. LABRANCHE	J.D. TATEM
CHECKED BY: RM	SAFETY
CCA	N/A
APPROVED BY:	COMMUNICATION

195113343

PROJECT NUMBER:
81018R

COOVER-CLARK JOB NUMBER:
201803

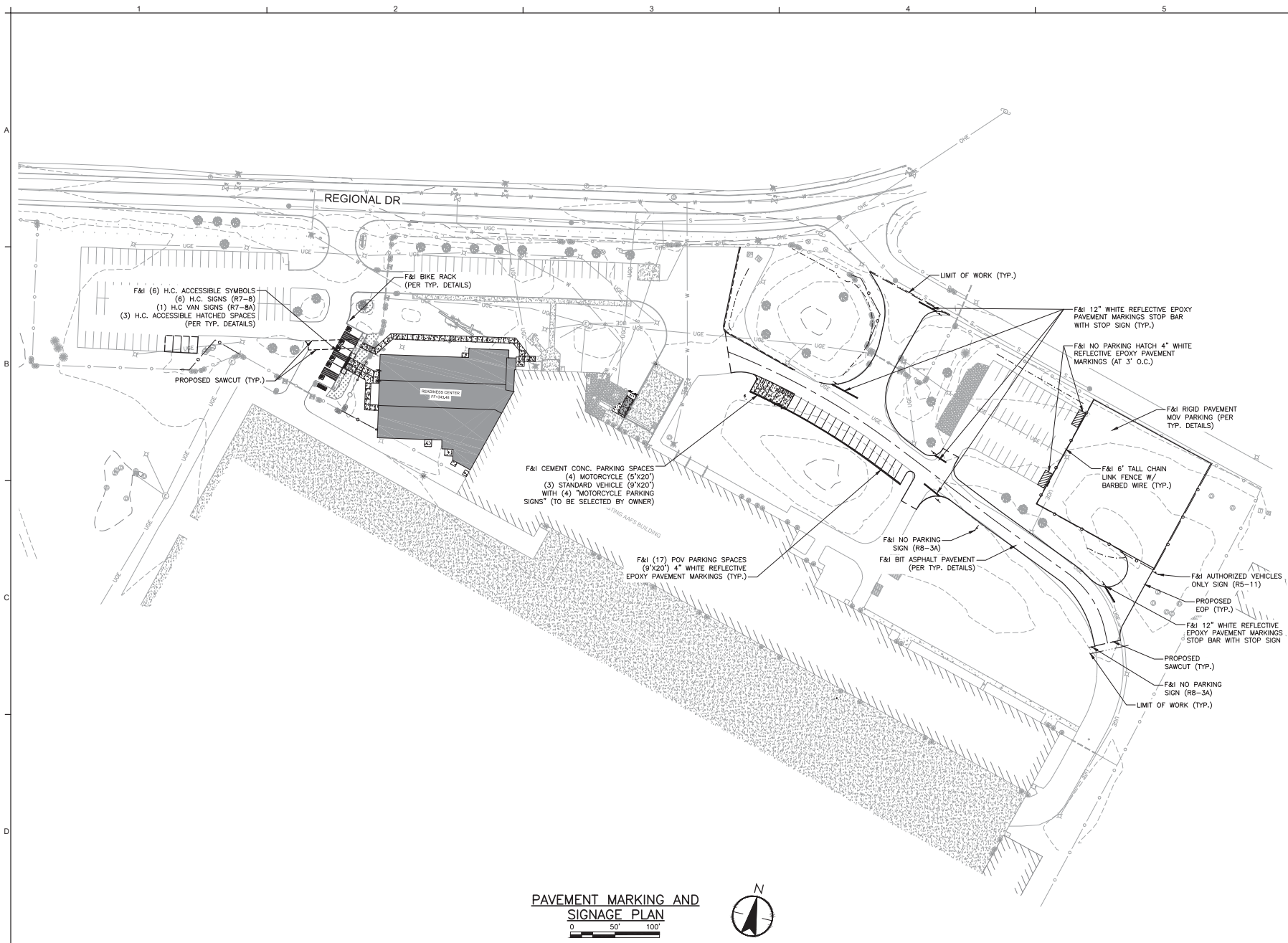
PROJECT TITLE:

CONCORD AVIATION
READINESS CENTER

DRAWING TITLE:

UTILITIES PLAN

DATE	PAGE NUMBER
JULY, 2020	C103
REV.	



PAVEMENT MARKING AND
SIGNAGE PLAN



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B. R/LIOFF	N.HARRING
DESIGNED BY	USING AGENCY
SAH	D. ANDRUS
BASE SHEET BY	FIRE CHIEF
E. BELISLE	N/A
DRAWN BY	ISO-ENVR. ENGR.
R. LABRANCHE	J.D. TATEM
CHECKED BY: RM	SAFETY
CCA	N/A
APPROVED BY:	COMMUNICATION
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PROJECT NUMBER: 81018R	
COOVER-CLARK JOB NUMBER: 201803	
PROJECT TITLE: CONCORD AVIATION READINESS CENTER	
DRAWING TITLE: PAVEMENT MARKINGS AND SIGNAGE PLAN	
DATE: JULY, 2020	PAGE NUMBER: C104
REV:	

NOTE: GC TO VERIFY QUANTITIES

KEY: ANT LIST
DECIDUOUS TREES

	BOTANIC NAME	COMMON NAME	SIZE	REMARKS
AS	Acer saccharinum	SUGAR MAPLE	3 1/2"-4"	6' CANOPY HT. MIN
TA	Tilia Americana	BASSWOOD	3 1/2"-4"	6' CANOPY HT. MIN

EVERGREEN SHRUBS

JSB	Juniperus sabrina var.	BROADMOOR JUNIPER	5 GAL	
-----	------------------------	-------------------	-------	--

LANDSCAPE LEGEND

	STRUCTURE
	NEW CONCRETE PAVING / SIDEWALK
	ROCK MULCH GRAVEL
	SEEDED GRASS
	NEW ASPHALT PAVING
	(E) CHAIN LINK FENCE
	NEW FENCE
	PROPERTY LINE
	PROJECT LIMIT
	ATFP STANDOFF

LANDSCAPE GENERAL NOTES

- FOR DRAINAGE WAYS AND CULVERTS, UTILITY LINES & SEWER, RE: CIVIL.
- FOR ALL PAVING, WALKS, CURBS, FENCING GRADING, RE: CIVIL.
- ALL SITE SECURITY DEVICES, RE: SPECS FOR TECHNICAL DATA, RE: ESS LOCATIONS AND ARRANGEMENTS, HAND HOLES AND CONDUIT, RE: CIVIL & ELEC.
- CONSTRUCTION FENCING AND EROSION CONTROL, RE: CIVIL.
- ALL ITEMS EXISTING U.N.O.
- KEEP SITE OPERATIONAL AT ALL TIMES.
- RESTORE ALL AREAS OF DISTURBANCE TO MATCH EXISTING CONDITIONS WITHIN THE PROJECT LIMIT AND CONTRACTOR LAYDOWN AREAS. LOAM AND RESEED AS REQUIRED AND PROVIDE ADDITIONAL GRAVEL AT EXISTING GRAVEL BED LOCATIONS
- RE SPEC FOR ALL DETAILED LANDSCAPE INFORMATION

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Designer	DESIGNED	USING
BASE SHEET	FIRE	
JL/ML	DRAWN	BIO-ENVR.
Checker	CHECKED BY	SAFETY
Approver	APPROVED	COMMUNICATION

PROJECT
81018R

COOVER-CLARK JOB
201803

PROJECT

CONCORD AVIATION
READINESS CENTER

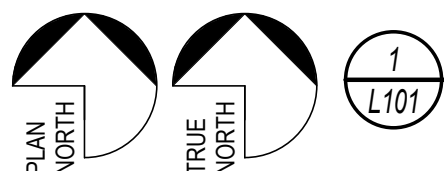
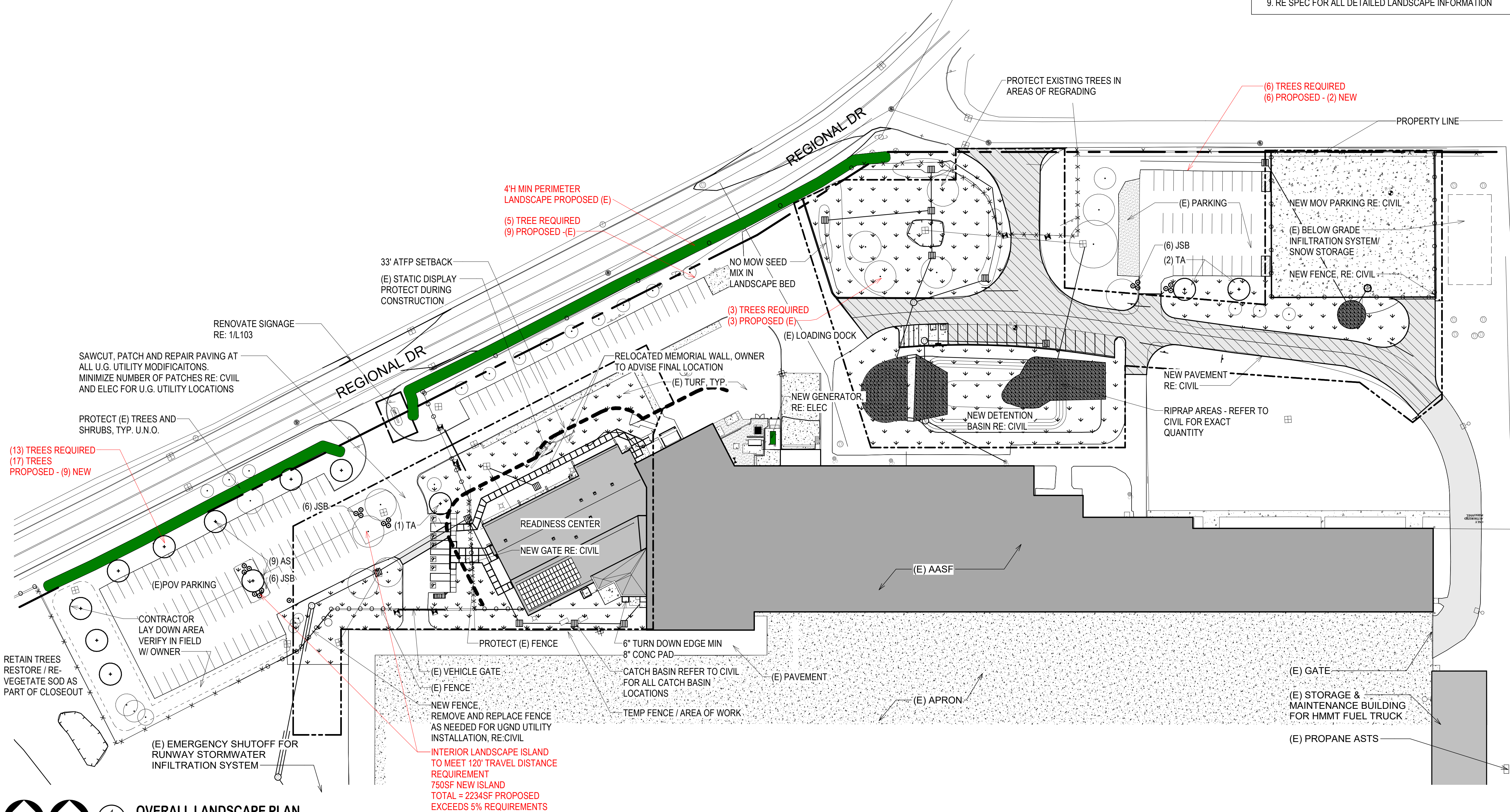
DRAWING

OVERALL LANDSCAPE PLAN

DATE
JULY 1, 2020

PAGE

L101

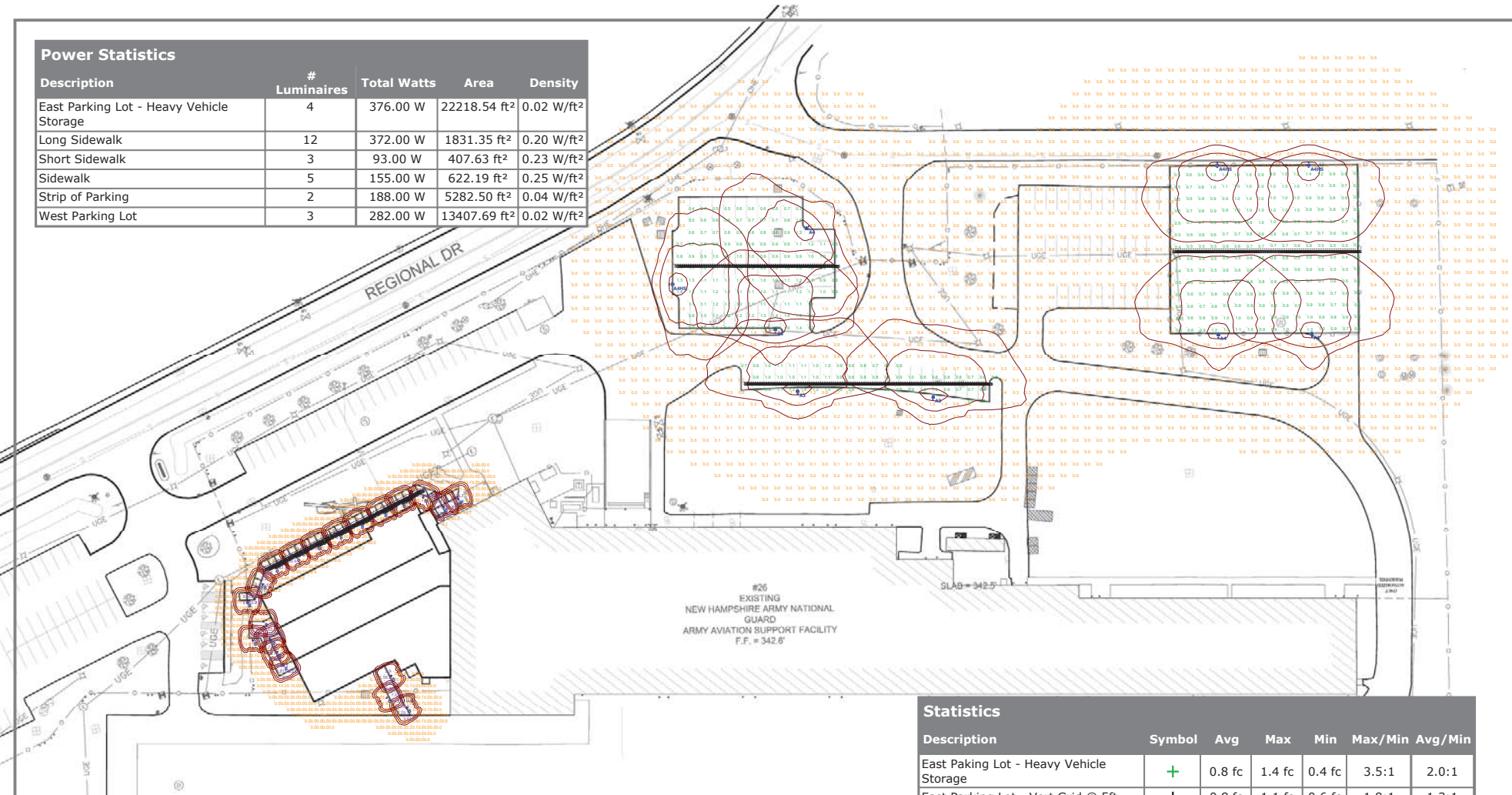


OVERALL LANDSCAPE PLAN

1
L101

Power Statistics

Description	# Luminaires	Total Watts	Area	Density
East Parking Lot - Heavy Vehicle Storage	4	376.00 W	22218.54 ft ²	0.02 W/ft ²
Long Sidewalk	12	372.00 W	1831.35 ft ²	0.20 W/ft ²
Short Sidewalk	3	93.00 W	407.63 ft ²	0.23 W/ft ²
Sidewalk	5	155.00 W	622.19 ft ²	0.25 W/ft ²
Strip of Parking	2	188.00 W	5282.50 ft ²	0.04 W/ft ²
West Parking Lot	3	282.00 W	13407.69 ft ²	0.02 W/ft ²



Schedule									
Symbol	Label	Quantity	Manufacturer	Catalog Number	Description	Lamp	Number Lamps	Filename	Lumens Per Lamp
	A3	2	Lithonia Lighting	KAD LED 40C 700 50K R3 MVOLT	KAD LED, 40 LED, 700mA MVOLT DRIVER, 5000K, TYPE 3 OPTICS; mounted at 30ft	LED	1	KAD_LED_40C_700_50K_R3_M VOLT.Jes	11068
	A4	4	Lithonia Lighting	KAD LED 40C 700 50K R4 MVOLT	KAD LED, 40 LED, 700mA MVOLT DRIVER, 5000K, TYPE 4 OPTICS; mounted at 30ft	LED	1	KAD_LED_40C_700_50K_R4_M VOLT.Jes	11081
	A4HS	3	Lithonia Lighting	KAD LED 40C 700 50K R4 MVOLT HS	KAD LED, 40 LED, 700mA MVOLT DRIVER, 5000K, TYPE 4 OPTICS WITH HOUSE SIDE SHIELDS; mounted at 30ft	LED	1	KAD_LED_40C_700_50K_R4_M VOLT_HS.Jes	8673
	B	20	Lithonia Lighting	DSXB LED 12C 700 50K ASY	D-SERIES BOLLARD WITH 12 5000K LEDS OPERATED AT 700mA AND ASYMMETRIC DISTRIBUTION; mounted at 3ft	LED	1	DSXB_LED_12C_700_50K_ASY.Jes	2349

Statistics

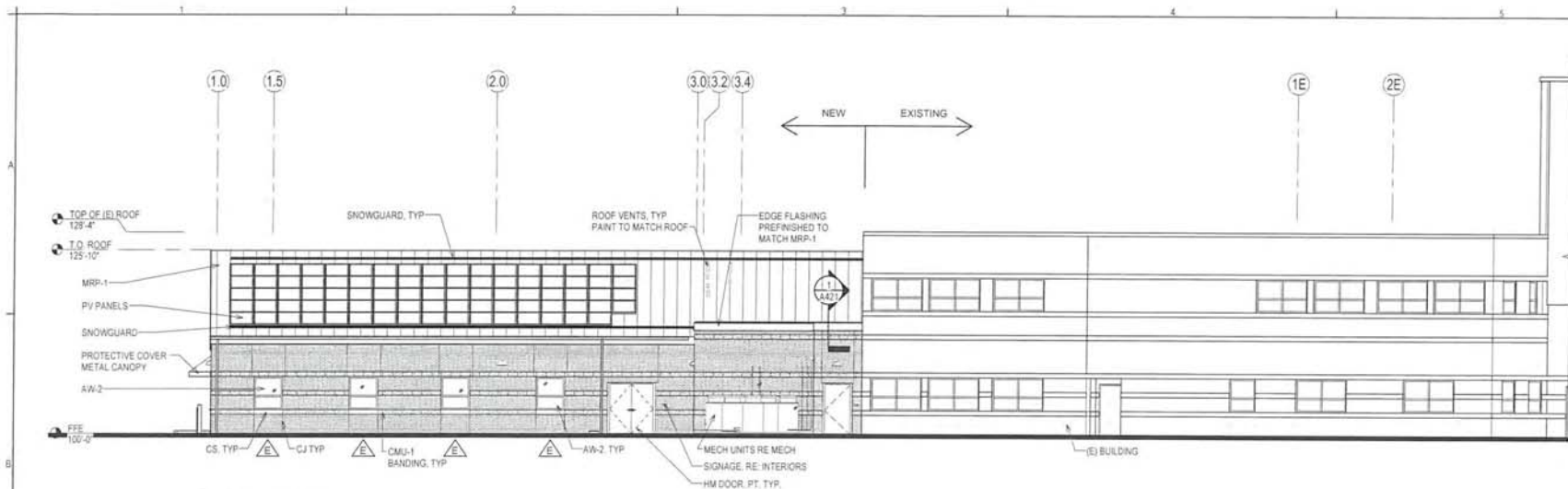
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
East Paking Lot - Heavy Vehicle Storage	+	0.8 fc	1.4 fc	0.4 fc	3.5:1	2.0:1
East Parking Lot - Vert Grid @ 5ft	+	0.8 fc	1.1 fc	0.6 fc	1.8:1	1.3:1
Long Side Walk - Vert Grid @ 1ft	+	10.5 fc	42.2 fc	0.2 fc	211.0:1	52.5:1
Long Sidewalk	+	19.6 fc	0.7 fc	0.7 fc	28.0:1	8.9:1
Outside of Parking Areas	+	0.1 fc	1.7 fc	0.0 fc	N/A	N/A
Outside of Sidewalks	+	0.8 fc	18.4 fc	0.0 fc	N/A	N/A
Short Sidewalk	+	5.8 fc	18.7 fc	1.5 fc	12.5:1	3.9:1
Sidewalk	+	6.7 fc	11.9 fc	1.4 fc	8.5:1	4.8:1
Strip of Parking	+	0.9 fc	1.4 fc	0.4 fc	3.5:1	2.3:1
Strip of Parking - Vert Grid @ 5ft	+	0.5 fc	0.7 fc	0.2 fc	3.5:1	2.5:1
West Parking Lot	+	1.0 fc	1.7 fc	0.4 fc	4.3:1	2.5:1
West Parking Lot - Vert Grid @ 5ft	+	0.9 fc	1.1 fc	0.6 fc	1.8:1	1.5:1



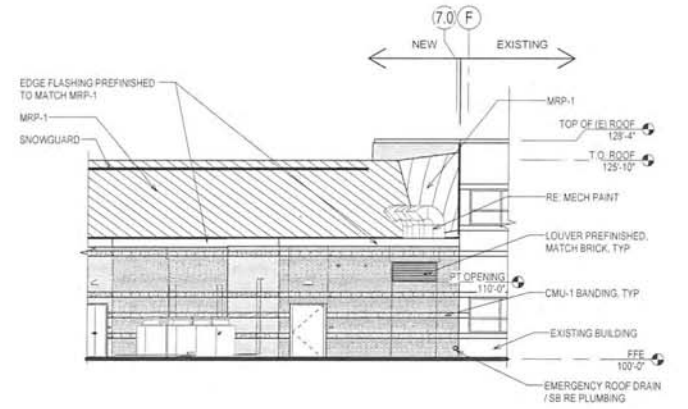
CONCORD READINESS CENTER ADDITION
Site Lighting Layout

Designer
Heidi G. Connors
Visible Light, Inc.
24 Shepley Terrace
Suite 6
Londonderry, NH 03052
Date
7/25/2019
Scale
1"=40'
Drawing No.

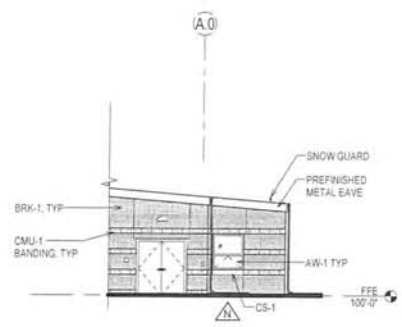
Summary
1 of 1
APRIL 2019



1 SOUTH ELEVATION
A302



2 SOUTH ENTRY ELEVATION
A302



3 NORTH EXIT ELEVATION
A302

PERCENTAGE OF GLAZING = 9.5%



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DATE PLOTTED	7/14/20
J.M.L.	DRAWN
DATE PLOTTED	7/14/20
CCC	APPROVED
DATE PLOTTED	7/14/20

PROJECT 81018R
COOVERCLARK JOB 201803

CONCORD AVIATION
READINESS CENTER

DRAWING
BUILDING ELEVATIONS

DATE JULY 1, 2020
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