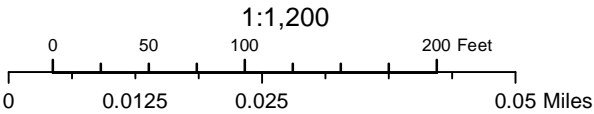
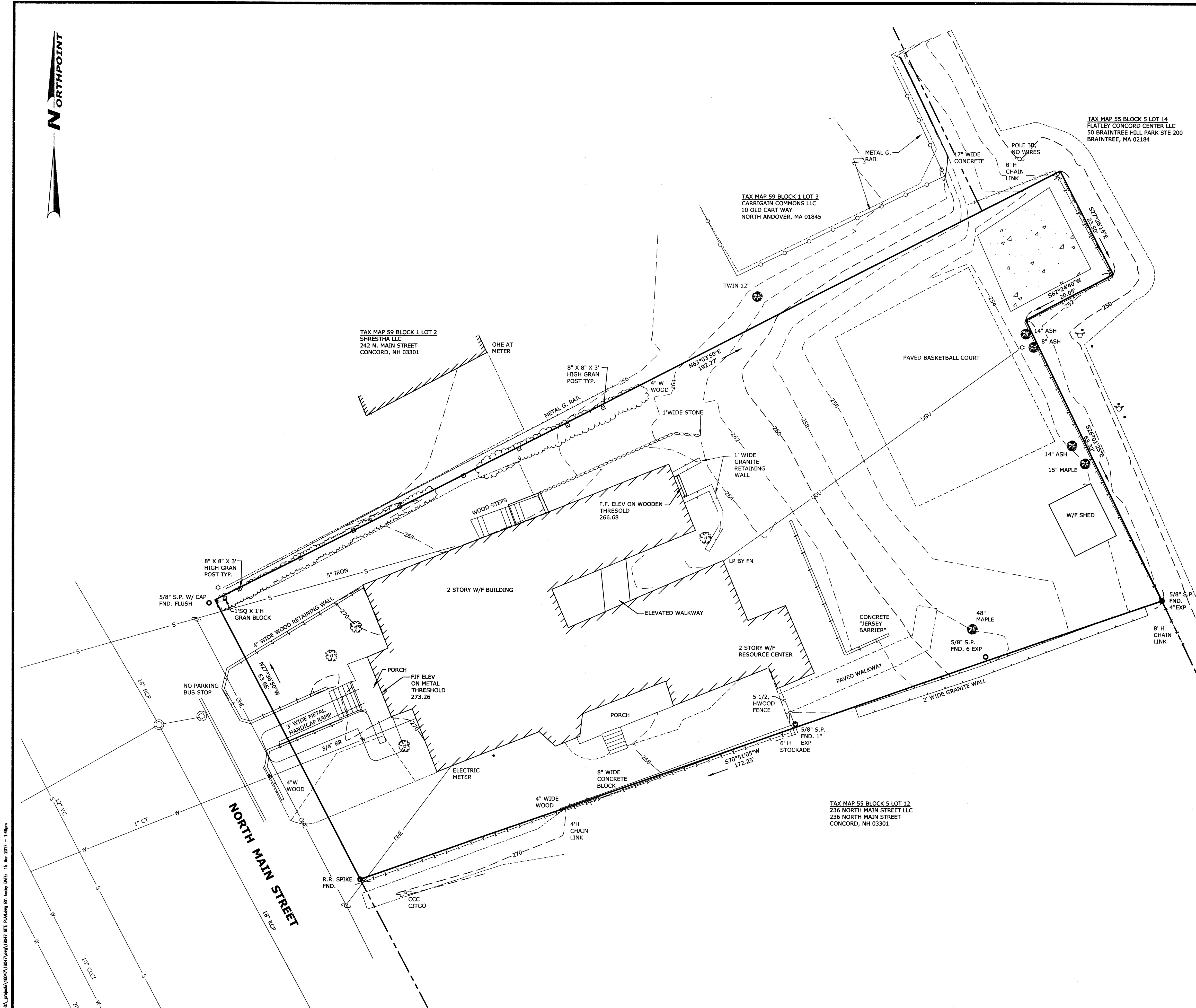


April 10, 2017

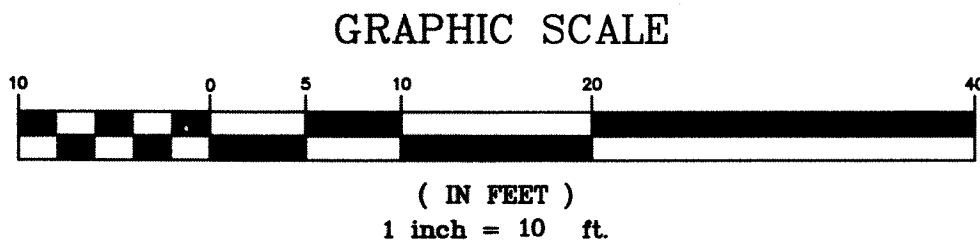




1. REFERENCE THE SUBJECT PARCEL OF LAND AS EXISTING LOT OF RECORD KNOWN AS CITY OF CONCORD MAP-BLOCK-LOT 55-5-13. THE SUBJECT PARCEL CONTAINS 13,405 SF OR 0.3077 ACRES.
2. THE PURPOSE OF THIS PLAN IS TO DEPICT EXISTING CONDITIONS ON SUBJECT PARCEL OF LAND.
3. THE SUBJECT PARCEL CONTAINS 13,405 SF OR 0.3077 ACRES.
4. OWNER OF RECORD: THOMSONBURGER, LLC
52 SNOW POND ROAD
CONCORD, NH 03301
V.3465 PG 2788 MCRD
5. THE SUBJECT PARCEL IS ZONED 'CU' URBAN COMMERCIAL DISTRICT AND IS SUBJECT TO THE FOLLOWING DIMENSIONAL REGULATIONS OF THE CITY OF CONCORD ZONING ORDINANCE:

	REQUIRED (CU)	EXISTING
MIN. LOT AREA:	1,500 SF	13,405 SF
MIN. BUILDABLE AREA:	6,250 SF	13,405 SF
MIN. LOT FRONTAGE:	100 FT	63.65 FT
MIN. FRONT SETBACK:	15 FT	16.5 FT
MIN. REAR SETBACK:	15 FT	2.5 FT (SHED)
MIN. SIDE SETBACK:	15 FT	8.6 FT
MAX. LOT COVERAGE:	80%	61 %
BUILDING HEIGHT:	45 FT	

NON-BUILDABLE AREA CALCS:
BLUFFS & RAVINES: 0 SF
EXISTING EASEMENTS: 0 SF
WETLANDS NOT IN EASEMENT: 0 SF
- 6 THE SUBJECT PARCEL IS NOT LOCATED WITHIN ANY OVERLAY DISTRICT.
7. EXISTING TOPOGRAPHIC AND BOUNDARY INFORMATION SHOWN ON THIS PLAN IS THE RESULT OF AN ACTUAL FIELD SURVEY PERFORMED BY RICHARD D. BARTLETT & ASSOCIATES, LLC IN THE FALL OF 2016.
8. HORIZONTAL DATUM IS BASED ON NEW HAMPSHIRE STATE PLANE COORDINATE SYSTEM NAD 83.
9. VERTICAL DATUM IS BASED ON NAVD 88.
10. THE UNDERGROUND UTILITIES DEPICTED HEREON HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND PLOTTED FROM EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES DEPICTED COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM THE INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND PORTION OF THE UTILITIES. ALL CONTRACTORS SHOULD NOTIFY, IN WRITING, ANY UTILITY COMPANY AND APPROPRIATE GOVERNMENTAL AGENCIES PRIOR TO ANY EXCAVATION WORK AND CALL DIG-SAFE AT 1-888-344-7233.
11. THE ENTIRE SUBJECT PARCEL FALLS IN ZONE X AS SHOWN ON THE FIRM - FLOOD INSURANCE RATE MAP - PANEL 0337E - COMMUNITY NUMBER 33013C0337E WITH AN EFFECTIVE DATE OF APRIL 19, 2010. ZONE X = AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.

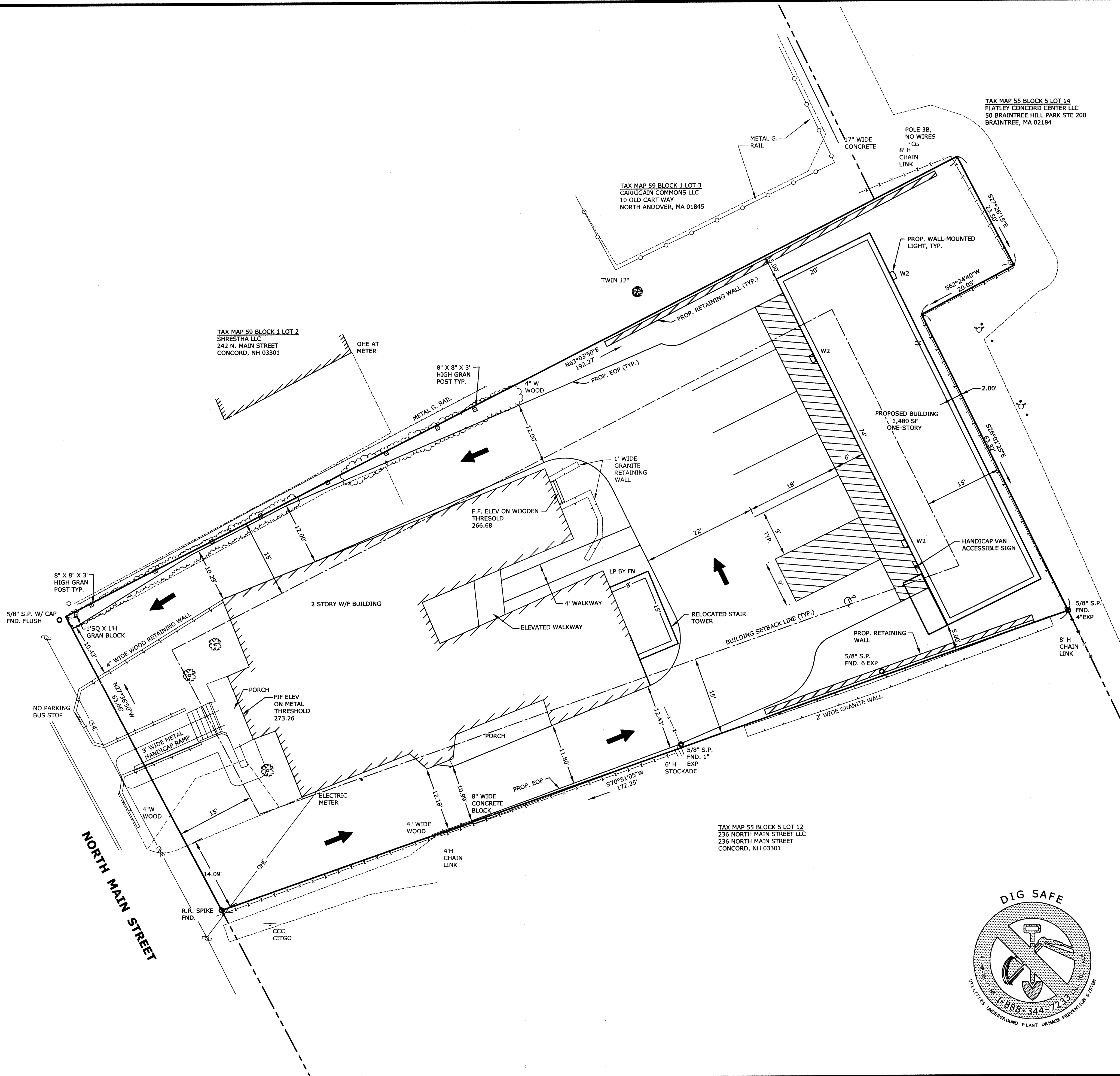


EXISTING CONDITIONS PLAN
PREPARED FOR:
CONCORD HOMELESS RESOURCE CENTER
238 NORTH MAIN STREET
CONCORD, NEW HAMPSHIRE

APPLICANT: CONCORD COALITION TO
END HOMELESSNESS
238 NORTH MAIN STREET
CONCORD, NH 03301

REVISIONS:		
NO.	DATE	DESCRIPTION

**NORTHPOINT
ENGINEERING, LLC**
Civil Engineering / Land Planning / Construction Services
119 Storrs St, Ste 201
Concord, NH 03301
Tel 603-226-1166
Fax 603-226-1160
www.northpointeng.com
DATE: 03/15/17
PROJ: 16047
SCALE: 1"=10'
SHEET: 1 of 7



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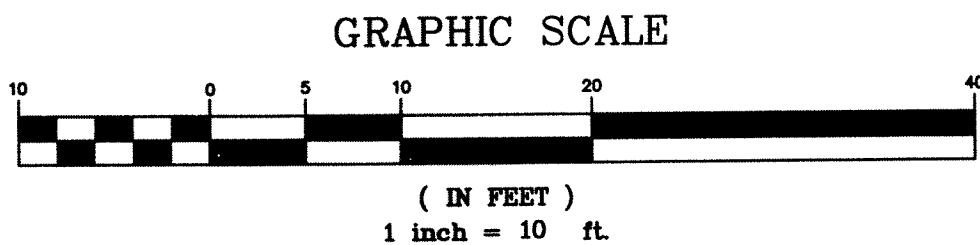
1. THE PURPOSE OF THIS PLAN IS TO DEPICT THE PROPOSED SITE IMPROVEMENTS ASSOCIATED WITH THE CONSTRUCTION OF A NEW BUILDING AND A NEW PARKING LOT ON THE ON THE SUBJECT PARCEL OF LAND. THIS EXISTING BUILDING IS TO REMAIN WITH SOME MODIFICATIONS.
2. REFERENCE THE SUBJECT PARCEL OF LAND AS EXISTING LOT OF RECORD KNOWN AS CITY OF CONCORD MAP-BLOCK-LOT 55-5-13. THE SUBJECT PARCEL CONTAINS 13,405 SF OR 0.3077 ACRES.
3. OWNER OF RECORD: THOMSONBURGER, LLC
52 SNOW POND ROAD
CONCORD, NH 03301
V.3465 PG 2788 HCRD
4. THE SUBJECT PARCEL IS ZONED 'CU' URBAN COMMERCIAL DISTRICT AND IS SUBJECT TO THE FOLLOWING DIMENSIONAL REGULATIONS OF THE CITY OF CONCORD ZONING ORDINANCE:

	REQUIRED (CU)	EXISTING	PROPOSED
MIN. LOT AREA:	1,500 SF	13,405 SF	13,405 SF
MIN. BUILDABLE AREA:	6,250 SF	13,405 SF	13,405 SF
MIN. LOT FRONTAGE:	100 FT	63.66 FT	63.66 FT
MIN. FRONT SETBACK:	15 FT	16.5 FT	16.5 FT
MIN. REAR SETBACK:	15 FT	2.5 FT (SHED)	2 FT PS
MIN. SIDE SETBACK:	15 FT	8.6 FT	5 FT
MAX. LOT COVERAGE:	80%	61%	80.1%
BUILDING HEIGHT:	45 FT		LESS THAN 45'

NON-BUILDABLE AREA CALCCS:
BLUFFS & RAVINES: 0 SF
EXISTING EASEMENTS: 0 SF
WETLANDS NOT IN EASEMENT: 0 SF
5. LOT COVERAGE CALCULATIONS:

EXISTING BUILDING AREA:	3,036 SF
PROPOSED BUILDING AREA:	1,480 SF
PAVEMENT AND CONCRETE:	6,222 SF
TOTAL PROPOSED IMPERVIOUS AREA:	10,738 SF (80.1%)
6. PARKING CALCULATIONS:

REQUIRED PARKING:	EXISTING BUILDING: 19 SPACES REQUIRED PROPOSED BUILDING: 12 SPACES REQUIRED TOTAL REQUIRED ONSITE PARKING = 31 SPACES
PARKING PROVIDED:	6 TOTAL SPACES, INCLUDING 1 HANDICAP ACCESSIBLE SPACES
7. ALL WORK PERFORMED SHALL CONFORM TO THE REQUIREMENTS OF THE LATEST EDITION OF THE CITY OF CONCORD CONSTRUCTION STANDARDS.
8. PRIOR TO START OF ANY CONSTRUCTION THE CONTRACTOR SHALL ARRANGE A PRE-CONSTRUCTION MEETING WITH THE ENGINEERING SERVICES DIVISION.
9. THIS PROJECT WILL BE CONSTRUCTED IN A SINGLE PHASE.
11. THE AREA OF LAND DISTURBANCE PROPOSED BY THIS PROJECT IS APPROXIMATELY 9,300-SF.
11. DURING PROJECT CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE AS-BUILT SKETCHES OF ALL UNDERGROUND UTILITIES PRIOR TO BACKFILL AND PROVIDE TO ENGINEER OR RECORD. ALL INVERTS AND ELEVATIONS SHALL BE BASED ON A SURVEYED BENCHMARK PROVIDED BY THE SURVEYOR OF RECORD THE SURVEYOR SHALL PROVIDE AN AS-BUILT OF THE SITE UTILIZING NH STATE PLANE GRID COORDINATES AND N.A.V.D. 88 DATUM AND SUBMIT TO THE ENGINEERING SERVICES DIVISION IN ELECTRONIC FORMAT FOR INCLUSION IN THE CITY GIS DATABASE.



SITE PLAN

PREPARED FOR:

CONCORD HOMELESS RESOURCE CENTER

238 NORTH MAIN STREET

CONCORD, NEW HAMPSHIRE

APPLICANT: CONCORD COALITION TO END HOMELESSNESS
238 NORTH MAIN STREET
CONCORD, NH 03301

REVISIONS:

NO.	DATE	DESCRIPTION

Northpoint ENGINEERING, LLC

Civil Engineering / Land Planning / Construction Services

119 Storrs St, Ste 201
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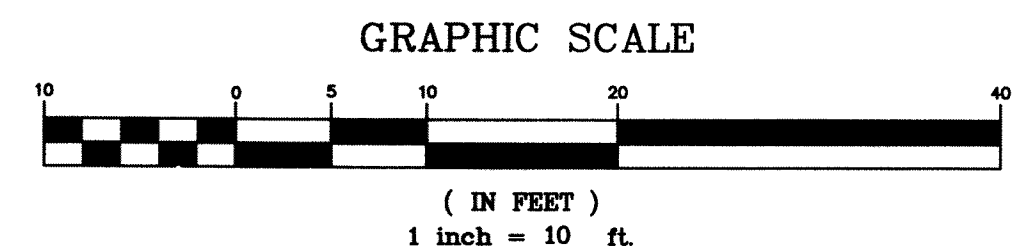
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PROJ: 16047
SCALE: 1"=10'
SHEET: 2 of 7





CONSTRUCTION NOTES:

1. PRIOR TO CONSTRUCTION, CONTACT DIG SAFE CENTER, TOLL FREE 1-888-344-7233. NEW HAMPSHIRE LAW REQUIRES NOTIFICATION AT LEAST THREE BUSINESS DAYS BEFORE DIGGING OPERATIONS START. IN AN EMERGENCY, CALL IMMEDIATELY.
2. LOCATION AND/OR ELEVATIONS OF EXISTING UTILITIES AND STRUCTURES SHOWN ON THIS PLAN SET ARE BASED ON PREVIOUS PLANS AND WHERE POSSIBLE VERIFIED IN THE FIELD. PRIOR TO ANY CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE EXACT SIZE, LOCATION, DEPTH AND EXISTENCE OF ALL EXISTING UNDERGROUND UTILITIES THAT ARE TO BE ACCESSED OR CROSSED DURING CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED IN WRITING IF ANY UTILITIES ARE FOUND TO BE INTERFERING WITH THE PROPOSED CONSTRUCTION AND APPROPRIATE REMEDIAL ACTION SHALL BE TAKEN BEFORE PROCEEDING WITH THE WORK.
3. ALL WORK SHALL CONFORM TO THE NHDOT STANDARDS AND SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE CITY OF CONCORD 2016 CONSTRUCTION STANDARDS AND DETAILS, AND THE CITY OF CONCORD SITE PLAN REGULATIONS, AND SHALL BE BUILT IN A WORKMAN LIKE MANNER IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.
4. PRIOR TO THE START OF CONSTRUCTION THE LIMITS OF CLEARING SHALL BE STAKED OUT. NO TREES SHALL BE CUT BEYOND THE LIMITS SHOWN ON THIS PLAN AND TEMPORARY FENCING SHALL BE INSTALLED AROUND THE LIMITS OF CLEARING PRIOR TO CLEARING TO ENSURE THAT NO ADDITIONAL TREES ARE DAMAGED DURING CONSTRUCTION. IMMEDIATELY AFTER CLEARING HAS OCCURRED, ALL TEMPORARY EROSION CONTROL MEASURES SHOWN ON THE EROSION CONTROL PLAN SHALL BE INSTALLED AND SHALL REMAIN IN PLACE AT ALL TIMES DURING CONSTRUCTION, AND UNTIL FINAL STABILIZATION HAS OCCURRED.
5. STORM DRAINAGE SYSTEM SHALL BE CONSTRUCTED TO LINE AND GRADE AS SHOWN ON THE PLANS. ALL PIPE MATERIAL SHALL BE HDPEP (OR APPROVED EQUAL) UNLESS OTHERWISE NOTED ON PLANS. CONSTRUCTION METHODS SHALL CONFORM TO NHDOT STANDARD SPECIFICATIONS.
6. ALL PROPOSED UTILITIES SHALL BE CONSTRUCTED UNDERGROUND.
7. ALL CONSTRUCTION DEBRIS, INCLUDING STUMPS, SHALL BE REMOVED FROM THE JOB SITE AND DISPOSED OF PROPERLY OFFSITE, UNLESS OTHERWISE APPROVED.
8. THE CONTRACTOR SHALL APPLY FOR AN EXCAVATION PERMIT (AVAILABLE FROM ENGINEERING SERVICES) PER CITY ORDINANCE ARTICLE 5-1-4 FOR WORK WITHIN THE CITY RIGHT-OF-WAY OR ON CITY OWNED PROPERTY. CONTACT THE CITY OF CONCORD ENGINEERING SERVICES DIVISION PRIOR TO CONSTRUCTION TO APPLY FOR THE PERMIT.
9. PROPOSED STORMWATER MANAGEMENT BASIN (SWMB#1) IS A SUBSURFACE INFILTRATION BASIN COMPRISED OF 24 STORMTECH SC-740 CHAMBERS WITHIN A STONE EXFILTRATION BED AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAILS AND CROSS-SECTIONS ON THE CONSTRUCTION DETAIL SHEETS. THE ROW OF CHAMBERS BETWEEN CB#1 AND DMH#1 SHALL BE INSTALLED AS AN ISOLATOR ROW PER MANUFACTURER RECOMMENDATIONS.
10. ALL AREAS DISTURBED DURING CONSTRUCTION, INCLUDING ALL CUT AND FILL SLOPES, SHALL BE PERMANENTLY STABILIZED WITH 6" OF LOAM AND SEED, PER THE TURF ESTABLISHMENT SCHEDULE. REFER TO THE EROSION CONTROL PLANS FOR ADDITIONAL REQUIREMENTS FOR TEMPORARY AND PERMANENT STABILIZATION PRACTICES IN CERTAIN AREAS OF THE SITE AS MAY BE APPLICABLE.
11. PRIOR TO START OF ANY CONSTRUCTION THE CONTRACTOR SHALL ARRANGE A PRE-CONSTRUCTION MEETING WITH THE ENGINEERING SERVICES DIVISION.
12. DURING PROJECT CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE AS-BUILT SKETCHES OF ALL UNDERGROUND UTILITIES PRIOR TO BACKFILL AND PROVIDE TO ENGINEER OR RECORD. ALL INVERTS AND ELEVATIONS SHALL BE BASED ON A SURVEYED BENCHMARK PROVIDED BY THE SURVEYOR OF RECORD. THE SURVEYOR SHALL PROVIDE AN AS-BUILT OF THE SITE UTILIZING NH STATE PLANE GRID COORDINATES AND N.A.V.D. 88 DATUM AND SUBMIT TO THE ENGINEERING SERVICES DIVISION IN ELECTRONIC FORMAT FOR INCLUSION IN THE CITY GIS DATABASE.



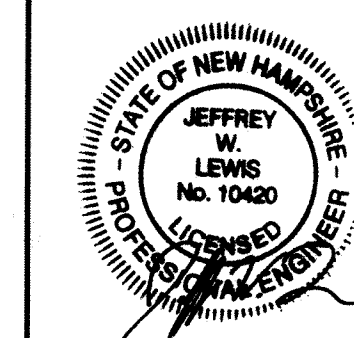
GRADING, DRAINAGE & UTILITIES PLAN

PREPARED FOR:

CONCORD HOMELESS RESOURCE CENTER

238 NORTH MAIN STREET
CONCORD, NEW HAMPSHIRE

APPLICANT: CONCORD COALITION TO
END HOMELESSNESS
238 NORTH MAIN STREET
CONCORD, NH 03301



REVISIONS:

NO. DATE DESCRIPTION



119 Storrs St, Ste 201
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DATE: 03/15/17
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SHEET: 3 of 7



EROSION CONTROL NOTES

1. THE SMALLEST PRACTICAL AREA SHALL BE DISTURBED DURING CONSTRUCTION, BUT IN NO CASE SHALL EXCEED 5 ACRES AT ANY ONE TIME BEFORE DISTURBED AREAS ARE STABILIZED.
2. ALL PERIMETER CONTROLS SHALL BE INSTALLED PRIOR TO ANY EARTH MOVING OPERATIONS.
3. ALL AREAS OF UNSTABILIZED SOIL SHALL BE TEMPORARILY STABILIZED AS SOON AS PRACTICAL BUT NO LATER THAN 45 DAYS OF INITIAL DISTURBANCE.
4. TEMPORARY EROSION CONTROL MEASURES SHALL BE INSTALLED IN STRICT ACCORDANCE WITH PROJECT PLANS. IN ADDITION, SIMILAR MEASURES SHALL BE INSTALLED WHERE AND WHEN THE FIELD CONDITION, OR FIELD OPERATION OF OF THE INDIVIDUAL SITE CONTRACTOR MAY WARRANT.
5. ALL TEMPORARY EROSION CONTROL MEASURES USED SHALL BE INSPECTED WEEKLY AND AFTER EVERY 0.5-INCH OR GREATER RAINFALL WITHIN A 24-HOUR PERIOD. ALL TEMPORARY EROSION CONTROL MEASURES SHALL BE CLEANED AND MAINTAINED AND OTHERWISE KEPT IN AN EFFECTIVE OPERATIONS MANNER THROUGHOUT THE CONSTRUCTION PERIOD.
6. ALL ROADWAYS AND PARKING LOTS SHALL BE STABILIZED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE. ALL CUT AND FILL SLOPES SHALL BE LOAMED AND SEEDED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.
7. ALL DISTURBED AREAS DESIGNATED TO BE TURF, SHALL RECEIVE A MINIMUM APPLICATION OF 4-INCHES OF LOAM (COMPACTED THICKNESS), PRIOR TO FINAL SEEDING AND MULCHING.
8. ALL SWALES AND DITCHLINES SHALL BE FULLY STABILIZED PRIOR TO HAVING STORMWATER DIRECTED TOWARDS THEM.
9. ALL SWALES AND DITCHLINES SHALL BE PERIODICALLY CLEANED OF DEPOSITED SEDIMENT SO AS TO MAINTAIN AN EFFECTIVE GRADE AND CROSS SECTION.
10. IN THE EVENT THAT DURING CONSTRUCTION OF ANY PORTION OF THIS PROJECT, A WINTER SHUTDOWN IS NECESSARY, THE CONTRACTOR SHALL STABILIZE ALL INCOMPLETE WORK AND PROVIDE FOR SUITABLE METHODS OF DIVERTING RUNOFF IN ORDER TO ELIMINATE SHEET FLOW ACROSS FROZEN SURFACES.
11. ALL PROPOSED VEGETATED AREAS WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED BY SEEDING AND INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 3:1, AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE, SECURED WITH ANCHORED NETTING, ELSEWHERE. THE PLACEMENT OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR ON FROZEN GROUND AND SHALL BE COMPLETED IN ADVANCE OF THAW OR SPRING MELT EVENTS
12. ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS.
13. AFTER NOVEMBER 15TH, INCOMPLETE ROAD OR PARKING SURFACES WHERE WORK HAS STOPPED FOR THE WINTER SEASON, SHALL BE PROTECTED WITH A MINIMUM OF 3 INCHES OF CRUSHED GRAVEL PER NHDOT ITEM 304.3, OR IF CONSTRUCTION IS TO CONTINUE THROUGH THE WINTER SEASON, BE CLEARED OF ANY ACCUMULATED SNOW AFTER EACH STORM EVENT.
14. DUST SHALL BE CONTROLLED BY THE USE OF WATER AS NECESSARY THROUGHOUT THE CONSTRUCTION PERIOD.
15. IN NO WAY ARE THOSE TEMPORARY EROSION CONTROL MEASURES INDICATED ON THESE PLANS TO BE CONSIDERED ALL INCLUSIVE. THE CONTRACTOR SHALL USE JUDGEMENT IN INSTALLING SUPPLEMENTARY EROSION CONTROL MEASURES WHERE AND WHEN SPECIFIC SITE CONDITIONS AND/OR CONSTRUCTION METHODOLOGIES MAY WARRANT.
16. AREAS HAVING FINISH GRADE SLOPES STEEPER THAN 3 : 1 SHALL BE STABILIZED WITH EROSION CONTROL MATS WHEN AND IF FIELD CONDITIONS WARRANT, OR IF SO ORDERED. EROSION CONTROL MATS SHALL BE INSTALLED TO CONFORM WITH THE RECOMMENDED BEST MANAGEMENT PRACTICE OUTLINED IN THE "STORMWATER MANAGEMENT AND EROSION AND SEDIMENT CONTROL HANDBOOK FOR URBAN AND DEVELOPING AREAS IN NEW HAMPSHIRE".
17. IN ORDER TO PROTECT WETLAND AREAS FROM SILTATION DURING CONSTRUCTION OF HOMES ON INDIVIDUAL LOTS, SILTATION FENCE SHALL BE INSTALLED UP GRADIENT OF DESIGNATED WETLANDS WHERE EXCAVATION IS PROPOSED TO OCCUR WITHIN 30-FEET OF SAID WETLANDS.
18. ALL CONSTRUCTION WITHIN 100 FEET OF ANY WETLAND SHALL BE UNDERTAKEN WITH SPECIAL CARE TO AVOID EROSION AND SILTATION INTO THE WETLANDS.
19. AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURED:
- BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED;
 - A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED;
 - A MINIMUM OF 3" OF NON-EROSIVE MATERIAL SUCH AS STONE OR RIPRAP HAS BEEN INSTALLED;OR
 - EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.

CONSTRUCTION SEQUENCE

1. CONTRACTOR TO NOTIFY DIG-SAFE 72-HOURS PRIOR TO COMMENCEMENT OF CONSTRUCTION.
2. CUT AND CLEAR TREES AND BRUSH ONLY WITHIN DESIGNATED LIMITS OF CLEARING AS NECESSARY TO FACILITATE PROPOSED CONSTRUCTION. ALL TREES, BRANCHES AND OTHER VEGETATIVE MATERIALS SHALL BE PROPERLY DISPOSED OF OFF SITE BY THE CONTRACTOR.
3. PRIOR TO COMMENCEMENT OF ANY GRUBBING OR EARTHMOVING OPERATIONS, ALL SPECIFIED PERIMETER CONTROLS AND STABILIZED CONSTRUCTION EXIT SHALL BE IN PLACE AS SHOWN ON THE PROJECT PLANS.
4. COMPLETE GRUBBING OPERATIONS. ALL STUMPS AND SIMILAR ORGANIC DEBRIS SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR. NATIVE ORGANIC SOIL MATERIALS SUITABLE FOR USE AS TOPSOIL SHALL BE STOCKPILED WITHIN AREAS OUT OF THE WAY OF OTHER CONSTRUCTIONS ACTIVITIES AND DRAINAGE FLOW. STOCKPILES SHALL BE TEMPORARILY SEEDED WITH WINTER RYE AND BE SURROUNDED WITH HAY BALES AND/OR FABRIC SILTATION FENCE IN ORDER TO PREVENT LOSS DUE TO EROSION.
5. PRIOR TO ROUGH GRADING ANY PORTION OF THE SITE, THE PERMANENT RETENTION BASINS AND DRAINAGE SWALES SHALL BE INSTALLED FOR ANY PORTIONS OF THE SITE THAT WILL DIRECT RUNOFF TO THE BASINS OR SWALES.
6. BEGIN EARTHMOVING OPERATIONS: PERMANENT DOWNSLOPE WORK SHALL BE PROTECTED FROM UPGRADIENT STORMWATER FLOW BY THE CONSTRUCTION OF TEMPORARY EARTHEN DIKES OR EXCAVATED SWALES.
7. INSTALL DRAINAGE SWALE SYSTEMS, DETENTION BASINS AND OTHER UTILITIES WORKING FROM LOW TO HIGH. INCOMPLETE WORK SHALL BE PROTECTED FROM SILTATION BY THE USE OF SILTATION BARRIERS AROUND SWALES UNTIL THE SITE HAS BECOME FULLY STABILIZED.
8. CONSTRUCT TEMPORARY CULVERTS AS NECESSARY TO FACILITATE CONSTRUCTION ACTIVITIES. ALL SUCH CROSSINGS SHALL BE PROTECTED WITH HAY BALE BARRIERS TO LIMIT EROSION.
9. CONSTRUCT CLOSED DRAINAGE SYSTEM, AND OTHER SUBSURFACE UTILITIES, AS APPLICABLE.
10. COMMENCE CONSTRUCTION OF ROADWAYS. PERFORM EXCAVATION ACTIVITIES REQUIRED TO ACHIEVE SUBGRADE ELEVATION. ALL EXCAVATED EMBANKMENTS, DITCHES, SWALES AND ROADWAY CROSS CULVERTS SHALL BE INSTALLED AND STABILIZED. ALL SWALES AND DITCHLINES SHALL BE PROTECTED FROM EROSION BY IMPLEMENTATION OF SILT FENCES AND/OR EROSION CONTROL MULCH BERMS AS SHOWN ON THE PROJECT PLANS. DIVERT STORMWATER RUNOFF THROUGH THE CULVERTS, OR OTHER MEANS NECESSARY PRIOR TO THE COMPLETIONS OF A FUNCTIONAL STORM DRAINAGE SYSTEM. SLOPES AND EMBANKMENTS SHALL BE STABILIZED BY TRACKING AND TEMPORARY SEEDING WITH WINTER RYE PRIOR TO TURF ESTABLISHMENT. ALL DITCHES AND SWALES SHALL BE STABILIZED PRIOR TO HAVING RUNOFF DIRECTED TO THEM.
11. COMPLETE CONSTRUCTION OF ROADWAY EMBANKMENTS BY ADDING APPROPRIATE BASE MATERIALS GRADED TO PROPER ELEVATION.
12. COMPLETE EXCAVATION /STABILIZATION GRADING ACTIVITIES. WHEN COMPLETE, IMMEDIATELY BEGIN TOPSOILING PROPOSED TURF AREAS USING STOCKPILED LOAM SUPPLEMENTED WITH BORROW LOAM, IF NECESSARY TO LEAVE A THICKNESS OF 4-INCHES OF FRIABLE LOAM.
13. APPLY TOPSOIL TO ROADWAY SLOPES AND OTHER AREAS DISTURBED BY CONSTRUCTION. TOPSOIL USED MAY BE NATIVE ORGANIC MATERIAL SCREENED SO AS TO BE FREE OF ROOTS, BRANCHES, STONES, AND OTHER DELETERIOUS MATERIALS. TOPSOIL SHALL BE APPLIED SO AS TO PROVIDE A MINIMUM OF 4-INCH COMPACTED THICKNESS.
14. FINE GRADE ALL FUTURE TURF AREAS AND HYDROSEED WITH THE SPECIFIED SEED MIXTURE IMMEDIATELY AFTER FINE GRADING IS COMPLETED.
15. PERFORM FINE GRADING OF ROADWAY BASE MATERIALS. INSTALL THE BINDER COURSE OF PAVEMENT OVER ALL DESIGNATED AREAS.
16. INSTALL THE SPECIFIED WEARING COURSE OF PAVEMENT OVER THE BINDER COURSE. COMPLETE INSTALLATION OF LANDSCAPING, SIGNAGE AND OTHER SITE AMENITIES.
17. CONTINUE TO MONITOR AND RECTIFY MINOR SITE AND SLOPE EROSION UNTIL ENTIRE SITE APPEARS TO BE COMPLETELY STABILIZED AND VEGETATED WITH A HEALTHY STAND OF TURF OR GROUND COVER. MAINTAIN SPECIFIED SILTATION/EROSION CONTROL MEASURES THROUGH ONE WINTER.
18. AFTER STABILIZATION REMOVE AND SUITABLY DISPOSE OF TEMPORARY EROSION CONTROL MEASURES.
19. MONITOR CONSTRUCTION ACTIVITIES ON INDIVIDUAL LOTS TO INSURE CONSTRUCTION ACTIVITIES ARE BEING PERFORMED IN SUCH A WAY AS NOT TO ENDANGER THE INTEGRITY OF ROADWAY EMBANKMENTS, STORMWATER SYSTEMS AND UTILITIES. ALL DRIVEWAYS ACROSS DITCHLINES SHALL HAVE CULVERTS INSTALLED IN ACCORDANCE WITH LOCAL REQUIREMENTS.
20. THE SMALLEST PRACTICAL AREA SHALL BE DISTURBED DURING CONSTRUCTION, BUT IN NO CASE SHALL EXCEED FIVE (5) ACRES AT ANY ONE TIME BEFORE DISTURBED AREAS ARE STABILIZED.
21. THE PROJECT SHALL BE MANAGED IN A MANNER THAT MEETS THE REQUIREMENTS AND INTENT OF RSA 430:53 AND CHAPTER AGR 38000 RELATIVE TO INVASIVE SPECIES.

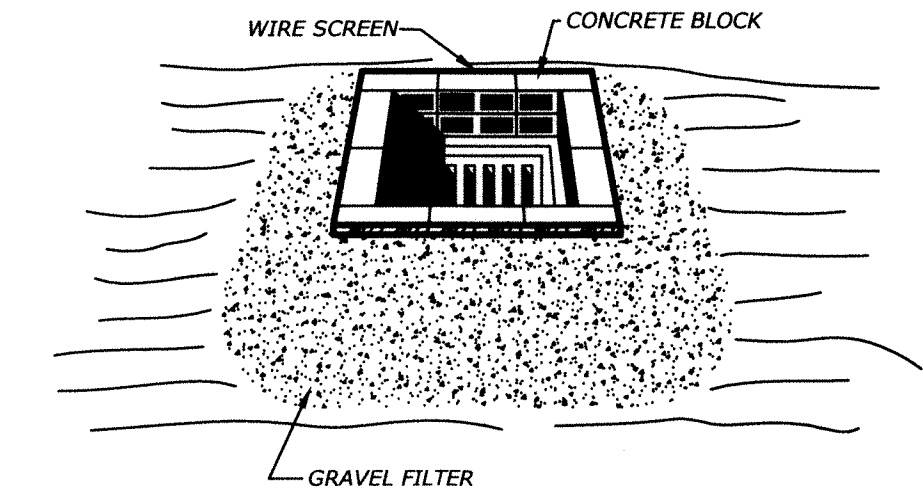
TURF ESTABLISHMENT SCHEDULE:

SEED BED PREPARATION:

1. PLACE 4-INCH (MINIMUM) COMPACTED THICKNESS OF CLEAN SUITABLE LOAM.
2. RAKE OUT LOAM TO FREE SOIL OF DEBRIS AND STONCS GREATER THAN 1-INCH IN DIAMETER.
3. FINE GRADE SURFACE AND SUPPLEMENT WITH LOAM WHERE NEEDED TO CREATE A UNIFORM SURFACE.
4. IF NEEDED, APPLY AGRICULTURAL LIMESTONE AT A RATE OF 11-POUNDS PER 1,000 S.F.

SEEDING SPECIFICATIONS:

1. SEED SHALL BE SPREAD UNIFORMLY BY BROADCASTING, DRILLING OR HYDROSEEDING. IF HYDROSEEDING, USE 4-TIMES THE RECOMMENDED RATE OF INOCULANT.
2. SEEDING FOR PERMANENT COVER SHALL OCCUR BETWEEN APRIL 15 AND OCTOBER 1. WHEN SEEDING BETWEEN MAY 15 AND SEPTEMBER 1, ALL AREAS SEEDED SHALL BE MULCHED WITH STRAW OR OTHER ACCEPTABLE MATERIAL AT A RATE OF 2-TONS PER ACRE.
3. SEED MIXTURE FOR LAWN AREAS SHALL CONTAIN 45% TALL FESCUE, 45% CREEPING RED FESCUE AND 10% REDTOP APPLIED AT A RATE OF 4.5-POUNDS PER 1,000 S.F.
4. ALL STEEP SLOPE AREAS (3 : 1 OR STEEPER) SHALL BE HYDROSEEDED WITH A SEED MIXTURE CONTAINING 25% PERENNIAL RYEGRASS, 50% CROWN VETCH AND 25% WILDFLOWER MIX APPLIED AT A RATE OF 4.5 POUNDS PER 1,000 S.F.
5. WETLAND AREAS SHALL BE SEEDED WITH SEPARATE NEW ENGLAND WETMIX.

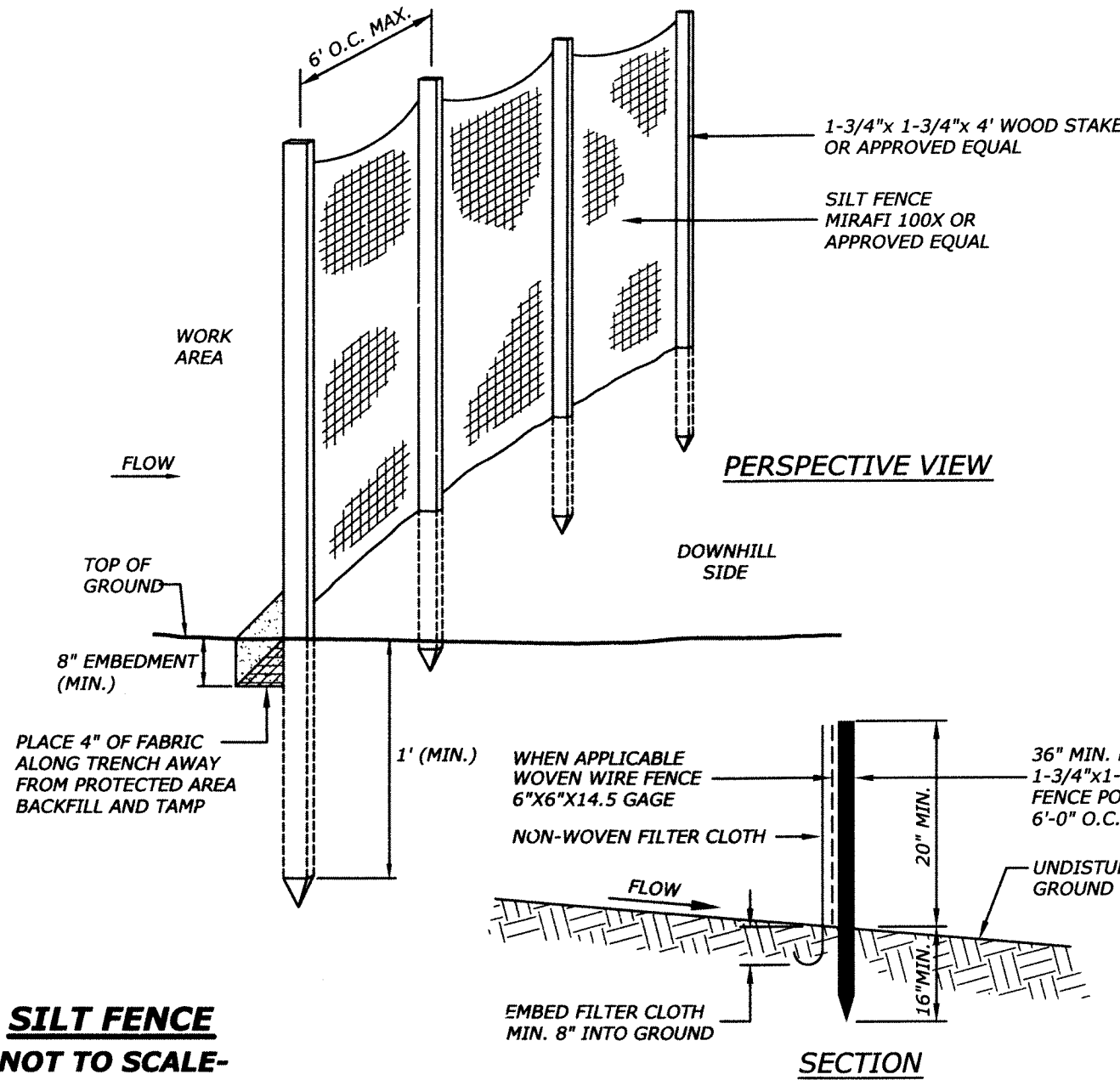


NOTES:

1. CONCRETE BLOCKS SHOULD BE PLACED LENGTHWISE ON THEIR SIDES IN A SINGLE ROW AROUND THE PERIMETER OF THE INLET. THE ENDS OF EACH BLOCK SHOULD BE ABUTTING. THE HEIGHT OF THE BARRIER CAN BE VARIED DEPENDING ON THE DESIGN BY STACKING VARIOUS COMBINATIONS OF DIFFERENT SIZED BLOCKS. THE BARRIER SHOULD BE A MINIMUM OF 12-INCHES HIGH AND A MAXIMUM OF 24 INCHES HIGH.
2. HARDWARE CLOTH OR WIRE MESH SHOULD BE PLACED OVER OPENINGS OF THE CONCRETE BLOCKS AND EXTENDED AT LEAST 12-INCHES AROUND THE OPENING TO PREVENT AGGREGATE FROM BEING TRANSPORTED THROUGH THE OPENINGS IN THE BLOCK.
3. SEWER STONE OR OTHER CLEAN COARSE AGGREGATE SHOULD BE PLACED AGAINST THE BLOCK TO THE TOP OF THE BARRIER.

BLOCK & GRAVEL DROP INLET SEDIMENT FILTER

-NOT TO SCALE-



SILT FENCE

-NOT TO SCALE-

CONSTRUCTION SPECIFICATIONS

1. THE GEOTEXTILE FABRIC SHALL MEET THE DESIGN CRITERIA FOR SILT FENCES.
2. THE FABRIC SHALL BE EMBEDDED A MINIMUM OF 8 INCHES INTO THE GROUND AND THE SOIL COMPACTED OVER THE EMBEDDED FABRIC.
3. WHEN APPLICABLE, WOVEN WIRE FENCE SHALL BE FASTENED SECURELY TO THE FENCE POSTS WITH WIRE TIE OR STAPLES. FILTER CLOTH SHALL BE FASTENED SECURELY TO THE WOVEN WIRE FENCE WITH TIES SPACED EVERY 24 INCHES AT THE TOP, MID-SECTION, AND BOTTOM.
4. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER, THEY SHALL BE OVERLAPPED BY 6 INCHES, FOLDED, AND STAPLED.
5. FENCE POSTS SHALL BE A MINIMUM OF 36 INCHES LONG AND DRIVEN A MINIMUM OF 18 INCHES INTO THE GROUND. WOOD POSTS SHALL BE OF SOUND QUALITY HARDWOOD AND SHALL HAVE A MINIMUM CROSS SECTIONAL AREA OF 3.0 SQUARE INCHES.

MAINTENANCE

1. SILT FENCES SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REPAIRS THAT ARE REQUIRED SHALL BE MADE IMMEDIATELY.
2. IF THE FABRIC ON A SILT FENCE SHOULD DECOMPOSE OR BECOME INEFFECTIVE DURING THE EXPECTED LIFE OF THE FENCE, THE FABRIC SHALL BE REPLACED PROMPTLY.
3. SEDIMENT DEPOSITS SHOULD BE INSPECTED AFTER EVERY STORM EVENT. THE DEPOSITS SHOULD BE REMOVED WHEN THEY REACH APPROXIMATELY ONE-THIRD THE HEIGHT OF THE BARRIER.
4. SEDIMENT DEPOSITS THAT ARE REMOVED OR LEFT IN PLACE AFTER THE FABRIC HAS BEEN REMOVED SHALL BE GRADED TO CONFORM WITH THE EXISTING TOPOGRAPHY AND VEGETATED.

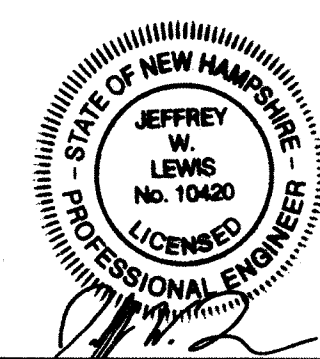
CONSTRUCTION DETAILS

PREPARED FOR:

CONCORD HOMELESS RESOURCE CENTER

238 NORTH MAIN STREET
CONCORD, NEW HAMPSHIRE

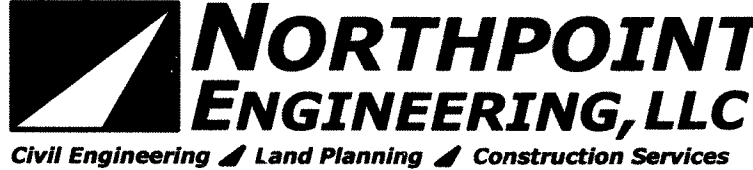
APPLICANT: CONCORD COALITION TO
END HOMELESSNESS
238 NORTH MAIN STREET
CONCORD, NH 03301



REVISIONS:

NO. DATE DESCRIPTION

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DATE: 03/15/17

PROJ: 16047

SCALE: NTS

SHEET: 7 of 7

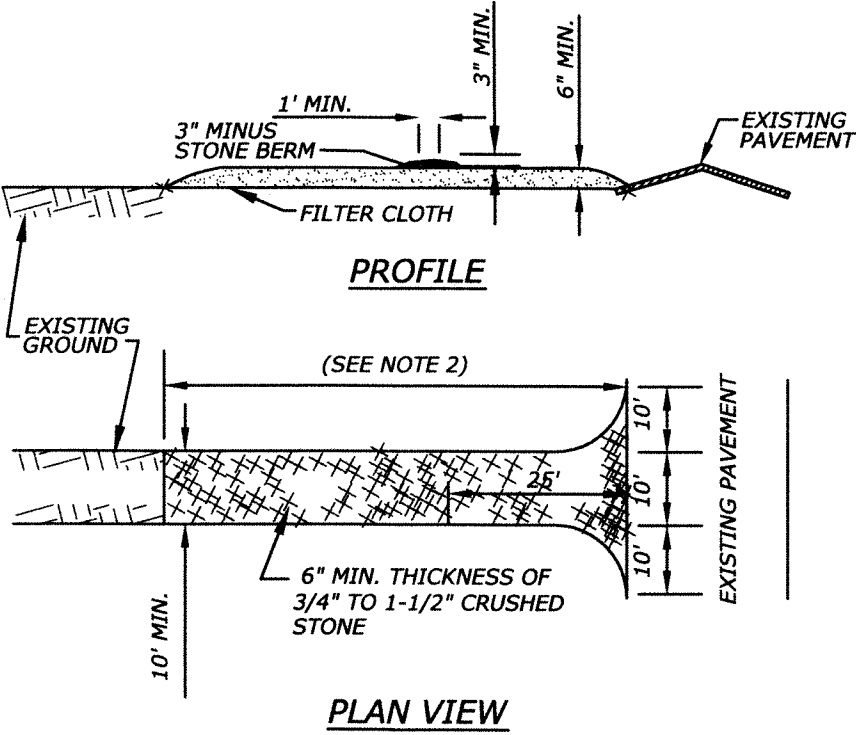
CONSTRUCTION SPECIFICATIONS

1. STONE FOR A STABILIZED CONSTRUCTION EXIT SHALL BE 1 TO 2 INCH STONE, RECLAIMED STONE, OR RECYCLED CONCRETE EQUIVALENT.
2. THE LENGTH OF THE STABILIZED EXIT SHALL NOT BE LESS THAN 50 FEET, EXCEPT FOR A SINGLE RESIDENTIAL LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY.
3. THE THICKNESS OF THE STONE FOR THE STABILIZED EXIT SHALL NOT BE LESS THAN 6 INCHES.
4. THE WIDTH OF THE EXIT SHALL NOT BE LESS THAN THE FULL WIDTH OF THE EXIT WHERE INGRESS OR EGRESS OCCURS OR 10 FEET, WHICH EVER IS GREATER.
5. GEOTEXTILE FILTER CLOTH SHALL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING THE STONE. FILTER CLOTH IS NOT REQUIRED FOR A SINGLE FAMILY RESIDENCE LOT.
6. ALL SURFACE WATER THAT IS FLOWING TO OR DIVERTED TOWARD THE CONSTRUCTION EXIT SHALL BE PIPED BENEATH THE EXIT. IF PIPING IS IMPRACTICAL, A BERM WITH 5:1 SLOPES THAT CAN BE CROSSED BY VEHICLES MAY BE SUBSTITUTED FOR THE PIPE.
7. THE EXIT SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOPDRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, WASHED, OR TRACKED ONTO PUBLIC RIGHT-OF-WAY MUST BE REMOVED PROMPTLY.
8. WHEELS SHALL BE CLEANED TO REMOVE MUD PRIOR TO EXIT ONTO PUBLIC RIGHTS-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.

MAINTENANCE

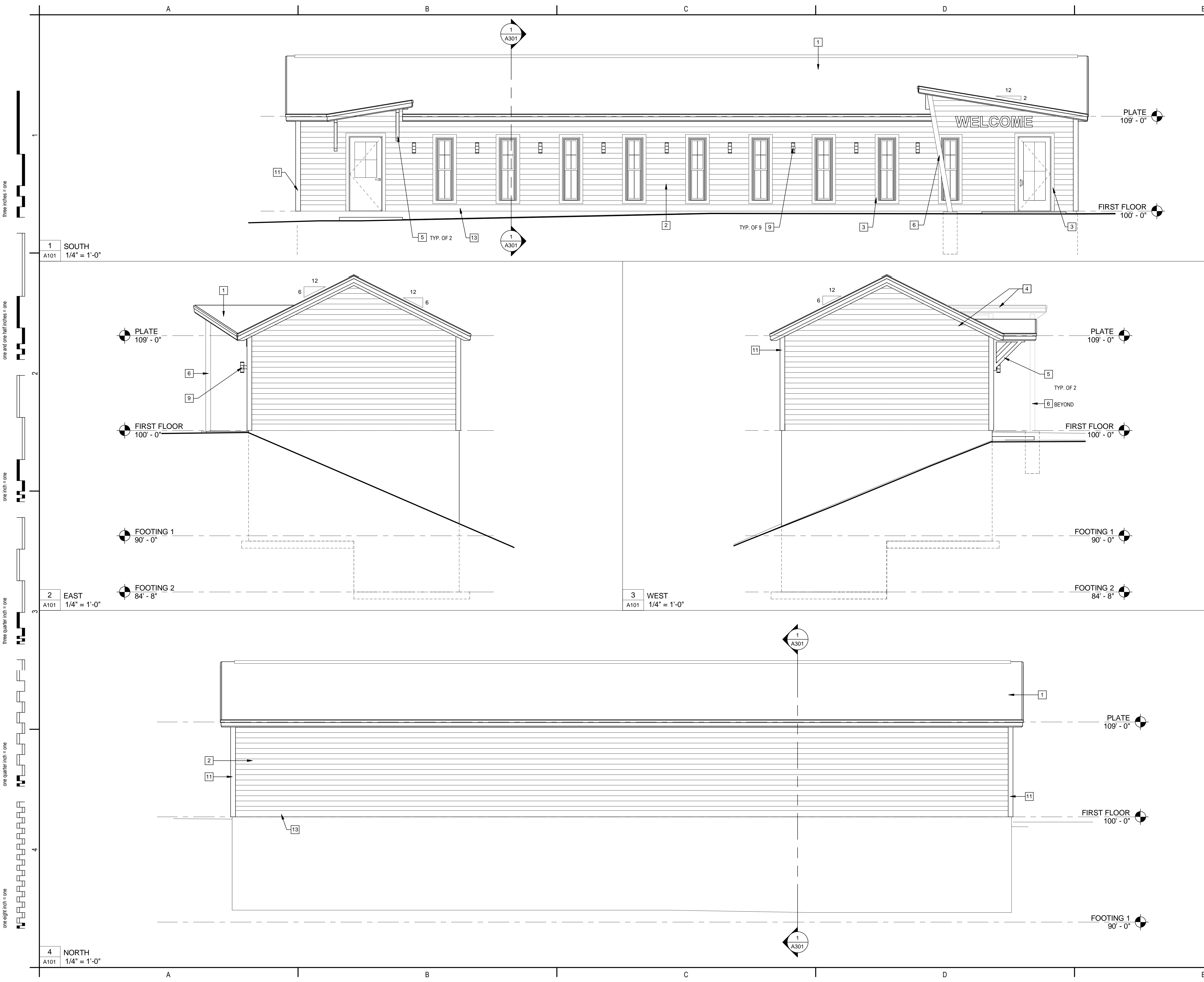
MUD AND SOIL PARTICLES WILL EVENTUALLY CLOG THE VOIDS IN THE GRAVEL AND THE EFFECTIVENESS OF THE GRAVEL PAD WILL NOT BE SATISFACTORY. WHEN THIS OCCURS, THE PAD SHOULD BE TOPDRESSED WITH NEW STONE. COMPLETE REPLACEMENT OF THE PAD MAY BE NECESSARY WHEN THE PAD BECOMES COMPLETELY CLOGGED.

IF WASHING FACILITIES ARE USED, THE SEDIMENT TRAPS SHOULD BE CLEANED OUT AS OFTEN AS NECESSARY TO ASSURE THAT ADEQUATE TRAPPING EFFICIENCY AND STORAGE VOLUME IS AVAILABLE. VEGETATIVE FILTER STRIPS SHOULD BE MAINTAINED TO INSURE A VIGOROUS STAND OF VEGETATION AT ALL TIMES.



STABILIZED CONSTRUCTION EXIT

-NOT TO SCALE-



GENERAL ELEVATION NOTES

1. REFER TO MECH DWGS FOR LOCATIONS OF LOUVERS AND VENTS. CONFIRM FINAL POSITION WITH ARCHITECT PRIOR TO ORDERING.

ELEVATION KEY NOTES

- 1 ASPHALT SHINGLES
- 2 CLAPBOARD W/ 6" EXPOSURE. COLOR: TBD
- 3 5/4"x4" PVC TRIM. COLOR: TBD
- 4 PVC FASCIA & SOFFIT TRIM. COLOR: TBD
- 5 36" X 36" X 6" SUPPORT BRACKET. COLOR: TBD
- 6 10 DEGREE ANGLED GALV. STEEL SUPPORT COLUMN, PTD.
- 7 SIGNAGE
- 8 EXTERIOR WALL PACK LIGHT
- 9 EXTERIOR DECORATIVE UP/DOWN WALL WASH LIGHT
- 10 42" GUARD RAIL
- 11 5/4"x6" PVC CORNER BOARD TRIM. COLOR: TBD
- 12 WOOD FRAMED DECK
- 13 5/4"x8 PVC WASHBOARD TRIM, PTD.

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FOR PRICING
NOT FOR
CONSTRUCTION

PROJECT TITLE /
**PROPOSED TEMPORARY
COLD WEATHER
SHELTER MODULAR
BUILDING**
238 N. MAIN STREET
CONCORD, NH

SCALE: AS NOTED DWN BY: KL
JOB #: 3213 CHK BY: JH

3/30/2017
ISSUED FOR PERMIT
100% REVIEW SET
REVISIONS:

EXTERIOR ELEVATIONS

A201
SHEET NUMBER: 6 OF 8
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three inches = one
one and one half inches = one
one inch = one
one quarter inch = one
one quarter inch = one
one quarter inch = one
one eighth inch = one

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2
3
4
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