



Heather Shank
City Planner

CITY OF CONCORD

New Hampshire's Main Street™
Community Development Department

Planning Board

January 19, 2021
Project Summary – Major Site Plan

Project: Grappone Mazda (2022-04)
Property Owner: HLF East, LLC
Address: 134 Manchester Street
Map/Block/Lot: 782Z / 40

Determination of Completeness:

Determine this application complete and set the public hearing for the February 16, 2022, Planning Board meeting. As part of the motion, indicate that the project meets the criteria for a Development of Regional Impact (DRI), per RSA 36:55, due to the location within the Aquifer Protection District that benefits the Town of Pembroke.

Project Description:

The applicant is proposing to demolish the existing building and construct a new 22,880 sf Mazda dealership with service bay areas, a 2,250 sf self-serve carwash facility, associated parking, and vehicle display areas at 134 Manchester Street in the Highway Commercial (CH) District.

Project Details:

Zoning: Highway Commercial (CH) District and Aquifer Protection Overlay District (AFP)

Existing Use: Vacant
Propose Uses: Car dealership with self-service carwash

Minimum Lot Area: 40,000 SF
Existing Lot Area: 173,152 SF (may change to 166,302 SF pending subdivision, See Comment 1.4 below)

Maximum Lot Coverage: 80%
Existing Lot Coverage: Not provided
Proposed Lot Coverage: 79.8%

Required Parking: 90, including 4 ADA spaces
Proposed Parking: 101, including 5 ADA spaces, plus 147 outdoor vehicular display spaces

Required Internal Landscaping: 5%
Provided Internal Landscaping: 5.6%

Total Trees Required: 55
Total Trees Provided: 52

1. General Comments

- 1.1 The following comments pertain to the 25-sheet site development plan set titled “Grappone Mazda, 134 Manchester Street, Concord, New Hampshire” prepared by Nobis Group, dated December 15, 2021, and revised Landscape plans prepared by WarrenStreet Architects dated January 6, 2022.
- 1.2 Architectural Design Review (ADR) is required for the Major Site Plan application. The Applicant is scheduled to go before the ADR Committee at their February 1, 2022, meeting.
- 1.3 See comments from the Engineering Division in a separate memo, dated January 10, 2022. Traffic Study review comments are forthcoming, pending revised traffic study.
- 1.4 The Applicant is proposing to annex and convey a portion of the property to the adjacent property. The Applicant shall submit a Subdivision Application for the lot line adjustment. Please see the Engineering Division memo regarding the proposed subdivision. Prior to final approval of the Site Plan, the subdivision plan shall be approved and recorded. The final Site Plans shall include the new lot lines as approved in the subdivision plan.

2. Conditional Use Permit(s)

- 2.1 The Applicant is requesting a Conditional Use Permit (CUP) in accordance with Section 28-7-7(f) of the Zoning Ordinance (ZO) to allow for the construction of a 40 ft wide driveway entrance where 28 ft is the maximum width.
 - Please see the attached narrative provided by the applicant describing how the criteria of the applicable sections of the Ordinance are met, including Section 28-9-4(b) (4) (ZO), which states the criteria for the Planning Board decision. Based on the findings of the traffic study, it is recommended that the driveway have two exit lanes.
 - Engineering Staff has concerns with the design of the driveway and size of the radii. Please see attached memo from Gary Lemay.
- 2.2 The Applicant is also requesting a CUP in accordance with Section 28-3-6(d)(4) *CUP Required for Certain Uses in the AP District* (ZO) for the construction of additional impervious surface area in the AP District and storage of regulated substances over 100 gallons.
 - Please see the attached narrative provided by the applicant describing how the criteria of the applicable sections of the Ordinance are met, including Section 28-9-4(b) (4) (ZO), which states the criteria for the Planning Board decision.
 - The Pembroke Planning Board will be reviewing the proposal at their January 25, 2022 meeting. The Conservation Commission will review the application at their February 9, 2022 meeting.
 - Per Section 28-3-6(d)(1) Performance Standards, the applicant shall provide additional information to demonstrate that stormwater infiltrated or discharged to the ground has been treated with the best available technology appropriate to mitigate all likely contaminants associated with the proposed use or activity, and provide a deicing management plan.

- The proposed application is increasing the amount of impervious area to over 133,025 sf (3.05 acres), or 79.8% of the site. The NH Stormwater Manual, Volume 1, Chapter 5 indicates that effective impervious cover values exceeding 25% are associated with significant impairment to receiving waters. The Applicant is providing more parking spaces than is required, is requesting relief to create a wider driveway, and is proposing drive aisles that exceed the maximum width, which will require a waiver. In order to support the CUP for increased impervious area in the AP District, Staff recommends that the Applicant reduce the amount of pavement on site by eliminating parking, reducing drive aisles, and providing additional landscape islands.

3. Site Layout Comments

- 3.1 Per Section 18.07 *Parking Lot Aisles*, of the Site Plan Regulations (SPR), maximum aisle widths shall not exceed 125% of the minimum aisle width, which is 30 ft for 2-way with 90 degree angle parking. The aisle widths far exceed the maximum in several locations. Applicant shall either reduce aisle widths or request a waiver.
- 3.2 Snow storage on the west and southern perimeter of the property are close to the property lines. Please provide additional information regarding how snow will be managed to avoid encroachment and melt runoff onto adjacent properties.
- 3.3 Please provide additional information on how the required customer and employee parking will be differentiated from vehicular display. Staff has concerns that vehicle display may take over required parking spaces.
- 3.4 Per Section 16.02(12) *Easements and Rights-of-way*, all easements shall be shown on the Site Plans. In addition, please confirm that the Applicant has coordinated with Consolidated Communications and Unutil regarding the existing utility easements. Will those easements be extinguished?
- 3.5 Per Section 21.02 *Sidewalks Required*, the applicant shall be required to construct a sidewalk along the frontage on Manchester Street. However, given that the City is proposing to reconstruct Manchester Street in the near future; staff is recommending that the Applicant set aside equivalent funds to be placed in a City escrow account to allow for the future construction of the sidewalk.
- 3.6 Per Section 20.03 (SPR) and 28-7-13(c) (ZO) *Design Standards for Loading Spaces*, please show the proposed loading spaces that meet the minimum requirements of 14 ft minimum width and 1000 sf minimum area.

4. Architecture, Lighting and Landscape Comments

- 4.1 Per Section 16.03 (SPR), revise the Architectural Elevations as follows:
- The Architectural elevations shall be signed or sealed by a NH Licensed Architect
 - The type and pitch of roofs shall be noted on the elevations. The size and spacing of all windows and door openings shall be noted on the elevations.
 - The proposed colors and materials for all exterior elements shall be provided.
 - Horizontal and vertical dimensions shall be provided on the elevations.
- 4.2 Per Article 28-7-7(j) (ZO) and 29.06 (SPR), lighting shall be designed to limit the increase in off-site illumination to a maximum of 0.2 foot candles and the side and rear lot lines. The lighting proposed exceeds 3 ft candles in some locations. Please revise.

In addition, lighting fixtures mounted higher than 15 ft above the ground shall be limited to cut-

off fixtures as defined by the Illuminating Engineering Society of North America. Please confirm the lighting meets this requirement. Further, please provide images of the proposed lighting fixtures for review by Architectural Design Review.

- 4.3 Per Section 27.07 *Required Landscape Improvements* (SPR), the majority of the trees in parking areas shall be large deciduous shade trees, and no more than 25% of the trees to be planted shall be classified as ornamental. In addition, Section 30.04 *Heat Island* (SPR) states that large deciduous trees in parking lots are encouraged to reduce heat island effects of large areas of pavement. As proposed, only 29% of the trees provided are classified as large deciduous shade trees. Red chokecherry is an ornamental tree (33% of trees provided), and Hornbeam and Sourwood are understory trees (38% of trees provided). The Applicant shall provide additional large deciduous shade trees and reduce ornamental and understory trees to meet the regulations.
- 4.4 Per Article 28-7-10(d) (ZO), required trees shall be planted no more than 50 ft apart in each contiguous area. No trees have been provided in the perimeter landscape area along Manchester Street. Applicant shall revise plan to include trees in this area meeting the requirement.
- 4.5 Revise the Planting Schedule as follows:
- *Prunus virginiana* is an ornamental tree, not a large deciduous tree. While staff appreciates that this tree provides high wildlife value, the fruit and resulting feces from feeding birds and the weak wooded branches may create a maintenance issue for a car dealership. Staff recommends the Landscape Architect reconsider this choice.
 - *Carpinus caroliniana* and *Oxydendrum arboreum* are understory trees, not large deciduous trees
 - *Baptisia australis* and *Coreopsis* are perennials, not shrubs
 - *Weigela* and *Boxwood* are shrubs, not low ground cover
- 4.6 The wildflower seed mix area around the car wash should be revised to exclude the car wash building and concrete walk to the building entrance. Will there be mulch, stone, or other treatment around the perimeter of this building?
- 4.7 Revise Landscape Specifications Section 3.3 on Sheet LA105 to match City standard details.

5. Technical Review Comments

- 5.1 Per Section 15.03(14) and 16.02(17) *Aquifers* (SPR), the Aquifer boundary shall be shown and labelled on the Existing Conditions Plan and Site Plans. In addition, please add a note to the Site Plan referencing the Spill Prevention, Control and Countermeasure (SPCC) Plan and deicing management plan.
- 5.2 Provide the appropriate tabulations to the Existing Conditions Plan, per Section 15.03(23) *Tabulations* (SPR), specifically existing impervious surface area in percentage and square foot.
- 5.3 Text labels are cut-off on a few of the plans, please revise accordingly.
- 5.4 Per Section 16.02(10) *Buildings and Structures*, exterior dimensions for the proposed structures shall be shown.

Prepared by: BAF

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CITY OF CONCORD
New Hampshire's Main Street™
Community Development Department

David Cedarholm, PE
City Engineer

MEMORANDUM

TO: Beth Fenstermacher, Assistant City Planner
FROM: Gary Lemay P.E., Associate Engineer
DATE: January 10, 2022
SUBJECT: Grappone Mazda Major Site Plan – Engineering Plan Review, 134
Manchester Street; Map 782Z, Lot 40; Project 2022-04

The Engineering Services Division (Engineering) has received the following items for review:

- Grappone Mazda, Site Plan, prepared by Nobis Engineering, dated December 15, 2021
- Grappone Mazda, Stormwater Management Report, prepared by Nobis Engineering, dated December 15, 2021
- Grappone Mazda, Traffic Study, prepared by Stephen G. Pernaw & Company, Inc., dated November 2021.

As a supplement to any comments offered by the Planning Division, Engineering offers the following design related comments. With subsequent submissions, the applicant shall provide a response letter that acknowledges or addresses each of these comments and discusses any additional changes to the plans.

Site Plans

Existing Conditions Plan

1. Per SPR 15.03 (13), please note that the property is not in the flood hazard district.
2. Per SPR 15.03 (23), please add the required tabulations or add an “existing” column to the site plan tabulations.

Demolition Plan (Sheet C-1)

3. The existing 1 ¼” water service shall be discontinued at the main rather than at the property line. The service line can be capped at the corporation stop and abandoned in place to limit the area of excavation.

Re: Review Comments (2022-004)
Grappone Mazda, 134 Manchester Street
Date: 1/10/2022

4. Please add a leader pointing at each monitoring well that shall be protected. What is the nature of the monitoring wells; are they scheduled for abandonment soon?
5. The clay line leading to DMH 1815 should also be removed within the City ROW, and the manhole inlet for this line should be closed off with brick.
6. One of the labels appears to be cut off at the top of the sheet. Please correct that on this sheet and any other similarly-impacted sheets.
7. Please add specifics (e.g., regrade, loam, seed) on how the City ROW will be restored where demolition or removal is taking place, such as regrading of the former driveways being removed.

Site Plan (Sheet C-2)

8. Some of the labels at the bottom of the sheet are cut off. Please adjust the drawing labels accordingly.
9. Please add exterior dimensions to the building, per SPR 15.04 (10).
10. Please add the dimension of the driveway across the street at Banks, per SPR 15.04 (11).
11. Within the plan view there are two notes that state “6,589 SF to be annexed to the City of Concord.” Please change this to “6,589 SF to be conveyed to the City of Concord by a permanent public highway right-of-way easement deed.” Please include a separate public highway right-of-way easement plan in a manner suitable for recording at the Merrimack County Registry of Deeds. In addition to the easement plan, an easement deed will also be recorded with the plan. Examples can be supplied upon request. Also, 6,589 SF may not be the final area for the new right-of-way. See comment below regarding the note that states “261 SF to be annexed to Lot 17.”
12. The permanent public highway right-of-way easement will become part of the right-of-way limits of Manchester Street. All setbacks, including landscape and building shall be measured from the new right of way line.
13. Within the plan view within the parcel, there is a 50’ setback line that corresponds to the current right-of-way line. Within the plan view there is also a note that states “Proposed 50’ building setback.” The leader for this note points to a line that does not stop at the side yard setbacks. Engineering suggests that only one 50’ setback line should be shown on the plan: the 50’ setback line from the new right-of-way line.
14. At the easterly end of the right-of-way to be conveyed to the City, the note for “landscape island (typ.)” is obscuring the boundary line.
15. As stated above, there is a note that states “261 SF to be annexed to Lot 17.” Engineering and City staff shall review the proposed configuration in conjunction with the conveyance of an easement for additional right-of-way. Due to the private

encroachment of improvements from 126 Manchester Street onto 134 Manchester Street, and the conveyance of land for additional right-of-way, this proposal may create unintended results. Two potential options for right-of-way conveyance have been attached to the Planning Report for reference. Engineering's recommendation is to proceed with Option 2. Please contact City staff for follow-up on this item.

- a. This item must be resolved by submission and approval of a subdivision plan prior to final Site Plan approval.
16. Bicycle parking shall be provided based on Section 30.03 of the Site Plan Regulations and the 2010 Bicycle Master Plan; and designed as follows:
- a. Located as close to the main entrance as practicable;
 - b. The City's preferred rack designs are an inverted "U" or a post and ring type where the center post is the same dimension as the rings.
 - c. Acceptable racks are the Square U Rack by MADRAX, Downtown Rack by Dero or similar.
 - d. Refer to the Association of Pedestrian and Bicycle Professionals (APBP) Bicycle Parking Guidelines or the City of Cambridge Bicycle Parking Guide (Appendix L, 2010 Concord Bicycle Master Plan) for additional information of acceptable rack types and layouts. A copy of the Bicycle Master Plan is available on the Planning Division section of the City's web page, www.concordnh.gov.
 - e. A detail of the selected rack and layout dimensions shall be included in the plans.
17. "No Parking" signs shall be provided at the front of the access aisles for the accessible parking spaces in accordance with Section Abfd 303.02(b)(3) of the NH AB Code and Part Bcr 303 (Changes or Updates to the International Building Code 2009, Amendment 9, 502.4.4.1) of the State Building Code.
18. Engineering would like to have a discussion with the Applicant to meet their turning movement needs while minimizing the driveway crossing distance for pedestrian and bike safety purposes. SPR 18.10 allows a maximum driveway radius of 25 feet, with exceptions made for industrial uses. Engineering can support a waiver request for SPR 18.10 and the associated driveway width CUP once we have been assured that the appropriate balance between driveway width, driveway radius flare (currently proposed at 36 feet), and pedestrian/bike safety has been met. For reference, the Banks site across the street appears to have 25-foot driveway radius flares.

Turning Motion Plan (Sheet C-2.1)

19. Engineering has some concern about the encroachment into the westbound lane of Manchester Street when turning into the driveway. Is this a conservative turning

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motion that may be refined to better analyze any encroachment into an adjacent City road lane?

20. Given the large truck that is shown successfully circulating the site, Engineering agrees that a turning motion plan for a firetruck is not necessary.
21. Please work with Engineering to ensure that the turning motions will work with the proposed new Manchester Street alignment and layout.
22. Confirm with Unitil and/or Consolidated Communications (whoever owns the utility pole) there is enough clear distance from the edge of the utility pole to the western edge of the proposed driveway.
23. Engineering has concerns with the proposed driveway alignment given the slight offset with the Banks driveway across the street, particularly once the proposed future center turn lane is constructed. The traffic study shall be revised to address the driveway alignment and location relative to the adjacent and opposing driveways.

Grading and Drainage Plan (Sheet C-3)

24. The proposed pitch across the driveway appears to be too flat. The catch basin in the driveway may need to be lowered to ensure positive drainage across the driveway.
 - a. Please add spot elevations across the driveway to ensure positive drainage (typical 1% min.).
 - b. The gutter line for the driveway should line up with the front (roadside) edge of CB 614.
25. Please add a label for DMH 18152 to this sheet and the other DMH in the street that 18152 ties into.
26. DMH 18152 is proposed to be re-cored at a similar elevation and the old corehole filled in; City specs require a minimum of 12" inside surface between holes and no more than 75% of a horizontal cross-section may be holes. If this cannot be met, the structure may need to be replaced with a new one.
27. Please add the diameters of all proposed manholes and existing manholes that will be modified.
28. Please add labels to each segment of pipe to make the material and size clear.
29. The segment of pipe tying into DMH 18152 must be RCP as it enters the City ROW.
30. What is DMH 8242 in the schedule for CB11 – should that say DMH 18152?

Utility Plan (Sheet C-4)

31. The sewer invert connection label at the top of the plan appears to be cut off (appears to say "Invert at Connection = 313.8")

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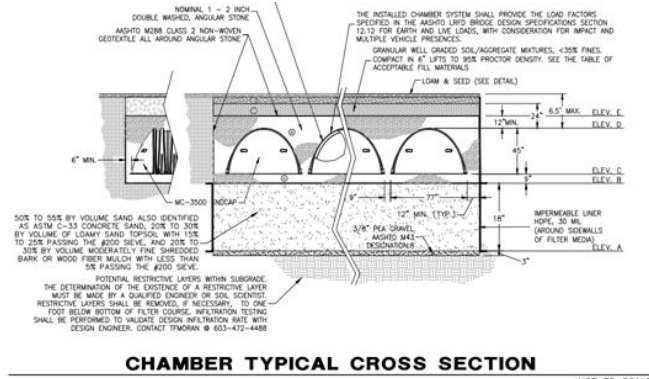
32. Per City Construction Standard 4.03.B.20, sewer service laterals greater than 6 inches in diameter must terminate in a sanitary sewer manhole. If may be possible to avoid a new manhole in the City ROW by tying into an existing nearby manhole.
33. The proposed sewer line is very close (<3 feet) to CB 614 at the end of the driveway; given the proposed sewer depth (approx. 10 feet) this will likely undermine the existing catch basin and adjacent structure. The line should be re-routed farther away from the structure, or the structures should be replaced after the new sewer line is run.
34. Please work with Engineering to ensure that the new proposed water and sewer utilities (as part of this site plan) do not conflict with the proposed utilities included in the Manchester Street Plans. Draft plans can be made available to the Applicant's Engineer. There is a proposed infiltration trench along the replacement drain line that is located between the water main and the sewer main that must be avoided.
35. What size is the sewer line exiting the oil/water separator from the main building? Please add labels describing the sewer sizes, where applicable.
36. Please specify that the water pipe used on site will be zinc-coated ductile iron pipe (typically abbreviated as ZCLDIP).
37. The fire service shutoff valve must be behind the domestic tap such that the fire service cannot be shut off without also turning off the domestic water in the building.
38. To help identify any potential utility conflicts near the City ROW, please provide the following utility profiles:
 - a. Please provide a sewer profile from the proposed connection to the City system to the first manhole on-site.
 - b. Please provide a water profile from the proposed connection to the City system to the first 45-degree fitting on-site.
 - c. Please provide a drain profile from the proposed connection to the City system to CB11.
39. Please add a callout (or a note) to the water service lines noting that, per City Construction Standard 5.03.D.14, there is a minimum 5-foot separation from the water service lines from all other utilities (except crossings).
40. Please note that contractors will need tracer wire for the short run of proposed 2" CTS; Engineering has received feedback from several contractors that it's easier to use copper instead of CTS for small runs to avoid the full tracer wire setup (with the proper grounding equipment).

Erosion Control Plan (Sheet C-4)

41. If feasible, please avoid the catch basin with the construction entrance by sliding it over slightly to avoid truck traffic damaging or rocking the catch basin frame.

Details

- 42. Please include a detail showing amended soil depths (if needed), with elevations for each of the chambers (use proposed elevations from sheet C-11)
- 43. Please include a detail for each of the oil-water separators.
- 44. There appear to be two water main/service details, one on sheet C-7 and one on Sheet C-8 (City detail). Please make sure the two details, if necessary, are consistent. The warning tape is shown as 12” above the main in the City detail and ~15” above the main in the other detail.
- 45. Per SPR 16.02 (22), please include a detail of the driveway, including a profile coming off of Manchester Street.
- 46. Please provide a cross-section detail of the infiltration chambers showing elevations of the chambers and any subgrades. See example photo below.



CHAMBER TYPICAL CROSS SECTION

NOT TO SCALE

	FILTRATION SYSTEM 1	FILTRATION SYSTEM 2	FILTRATION SYSTEM 3	FILTRATION SYSTEM 4
ELEV. A				
ELEV. B				
ELEV. C				
ELEV. D				
ELEV. E				
ELEV. OUTLET				
ELEV. INLET				

NOTE: CONTRACTOR TO CONFIRM THAT A PERCOLATION RATE OF 5 MIN./IN. IS MAINTAINED AT END OF CONSTRUCTION

Stormwater Management Report

- 47. What was the infiltration rate found for the soils on site?
- 48. What is the assumed infiltration rate for the underground infiltration chamber?
- 49. Please provide an electronic version of the HydroCAD files for Engineering to review as part of subsequent submissions. Engineering reviews the files to understand and check modeling assumptions, infiltration rates, etc.
- 50. Please confirm the design infiltration rate reflects a safety factor of two from field conditions. Additionally, the design infiltration rate shall be no more than 5 inches per hour after applying the safety factor.
- 51. Per SPR 12.03 (6), the soil scientist that prepared the soils map shall sign and seal the plan mapping the soils.

Traffic Study

52. Traffic study comments will be provided under separate cover.

General Comments

53. Please add a text scale to the drawings, per SPR 12.02.
54. The Applicant should be aware of the Aquifer Protection Overlay Zone restrictions in City Zoning Ordinance 28-3-6 (d), including the requirement that chloride salt use for deicing shall be restricted to the minimum amount needed for public safety.
55. Per Site Plan Regulation 21.02, sidewalks are required along the frontage of the redeveloped property; alternatively, the Board may require the applicant to set aside equivalent funds to be placed in a City escrow account to allow for future construction of a sidewalk. Given the City plans to reconstruct Manchester Street with a sidewalk fronting this property, Engineering feels that an escrow account to be used as part of the Manchester Street reconstruction would be more appropriate in this instance.
56. An FAA form 7460 may be necessary depending on the use of cranes during construction. Please submit a copy of the approved form to Engineering or indicate why the form is not needed.
57. Per SPR 19.05, Engineering recommends the site design include design provisions for interconnected parking lots between this property and adjacent lots along Manchester Street. If an interconnection cannot be made at this time, then the Applicant should propose easement locations for future interconnections for when the adjacent properties are redeveloped.
58. Engineering encourages, but does not require, the following wildlife-friendly erosion control practices:
- a. Use temporary erosion and sediment control products that either do not contain netting, or that contain netting manufactured from 100% biodegradable non-plastic materials such as jute, sisal, or coir fiber. Degradable, photodegradable, UV-degradable, oxo-degradable, or oxo-biodegradable plastic netting (including polypropylene, nylon, polyethylene, and polyester) are not equivalent alternatives. Netting used in these products should have a loose-weave wildlife-safe design with movable joints between the horizontal and vertical twines, allowing the twines to move independently and thus reducing the potential for wildlife entanglement.
 - b. Avoid the use of silt fences reinforced with metal or plastic mesh or if possible recommend the use of erosion control berms.
 - c. When no longer required, temporary erosion and sediment control products should be removed promptly from the project site.

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- d. Use nonwoven coir fabric when a surface fabric treatment is required for erosion control and stabilization, such as 100% biodegradable coconut fiber mat or equal as reviewed and approved by the project design engineer.
- e. Use woven coir fabric when site conditions warrant. The outer layer of woven coir fabric should be a high strength, continuously woven mat (i.e., without seams) and made of 100% coconut fiber.

State/Federal Permits

The project will require the following state and/or federal permit(s) associated with the site design:

- EPA Construction General Permit – Notice of Intent
- FAA Obstruction Evaluation/Airport Airspace Analysis (OE/AAA) – Notice of Proposed Construction or Alteration (Form FAA 7460-1)
- NHDES Alteration of Terrain Permit
- NHDES Registration and Notification for Storm Water Infiltration to Groundwater
- NHDES Sewer Connection Permit

Per Site Plan Regulation 13.02 (8), a copy of the State and Federal permit(s) shall be submitted to the City prior to final approval.

Post-Approval/Pre-Construction Items

The following items will need to occur prior to the start of construction (unless otherwise noted):

1. Prior to engineering sign-off on the building permit, the applicant/contractor shall:
 - a. Set up a pre-construction meeting with the Engineering Services Division to discuss construction requirements, site inspections, associated fees, schedules, etc. Any Engineering permits will not be authorized (unless explicitly stated otherwise) until final revised plans have been submitted and approved to the satisfaction of Planning and Engineering.
 - b. Provide PE-stamped backup information on the sizing of the proposed oil/water separator. Information provided shall include size and dimensions of the proposed separator and anticipated daily average flow (gpd) to the grease trap.
 - c. Provide a copy of FAA Form 7460.
 - d. Provide and record a City-approved public highway right-of-way easement at the Merrimack County Registry of Deeds for the widening of Manchester Street.
 - e. Complete the lot line adjustment with the adjacent property to the west (referred to as Lot 17 in the plans) concurrent with or prior to this work.

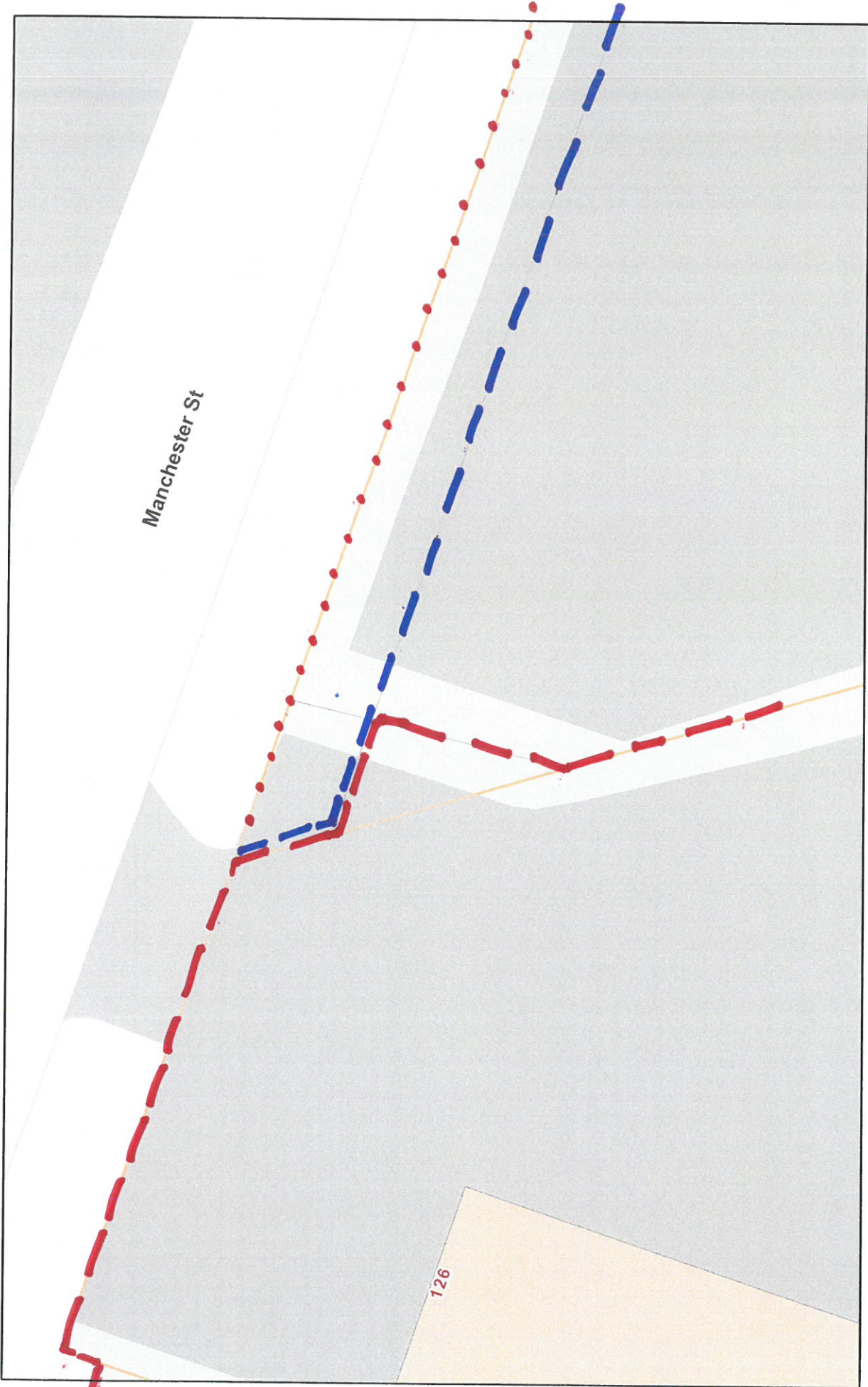
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2. A Demolition Permit will be required from the Code Administration Division for removal of the existing building(s).
3. The following permit(s) will need to be obtained from the Engineering Services Division:
 - a. Driveway Permit
 - b. Excavation Permit (for work within the ROW)
 - c. Utility Connection Permits (sewer and water services; storm drain connections)
4. The contractor shall submit a Temporary Traffic Control Plan (TTCP) for all work in and adjacent to the City ROW that will require lane closures or occur adjacent to the edge of road. TTCP must be submitted to Engineering for review and approval a minimum of two weeks prior to the pre-construction meeting.
5. Per Site Plan Regulation 27.11, establish a financial guarantee (letter of credit, or cash deposit) for site stabilization. The surety amount for this project has been set at \$30,000 based on a 3.5 ac disturbance area. The surety shall be established prior to scheduling the pre-construction meeting.
6. Establish a performance surety (bond, letter of credit, or cash deposit) for work within the City right-of-way per SPR 13.02 (5). An engineer's cost estimate, prepared by the Applicant and based on the current NHDOT weighted average unit prices, shall be submitted a minimum of two weeks prior to scheduling the pre-construction meeting. The surety shall be established at least one week prior to the pre-construction meeting.
7. Per Site Plan Regulation 36.24, the Applicant is responsible for paying engineering inspection fees to ensure work is consistent with City standards and the approved plan set. An advanced deposit must be established for all anticipated site construction inspection fees.
 - a. The deposit amount is determined by Engineering based on the project schedule and estimated services; actual inspection fees are based on actual services rendered (i.e., hourly billing rate).
 - b. Prior to scheduling the pre-construction meeting, a template estimating the initial fee deposit (available upon request to engineering) shall be filled out by the Applicant for review by Engineering, as well as a project schedule and itemized cost estimate for use in establishing the deposit amount.
 - c. The deposit shall be submitted at least a week prior to the pre-construction meeting.
8. Water and Sewer Investment Fees will need to be paid (as part of the utility connection permit process).
9. Retaining wall design drawings (stamped by a licensed structural engineer) shall be submitted to Engineering for proposed retaining walls that are greater than 4 feet high.

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In addition, walls greater than 48 inches require a Building Permit from the Code Administration Office.

10. Shop drawings/submittals shall be submitted to Engineering for the proposed water, sewer, and drainage improvements, and for proposed improvements within the ROW.
11. Prior to the construction of any future site improvements the applicant/owner shall consult the Planning Division to determine if Site Plan Approval will be required.
12. Per Site Plan Regulation 12.09, prior to issuance of a Certificate of Occupancy (CO), the contractor shall submit digital as-built drawings that are to the satisfaction of Engineering and conforming to the Engineering as-built checklist. A copy of the as-built drawing requirements are available on the Engineering website.



December 20, 2021

1:240

40 Feet

20

10

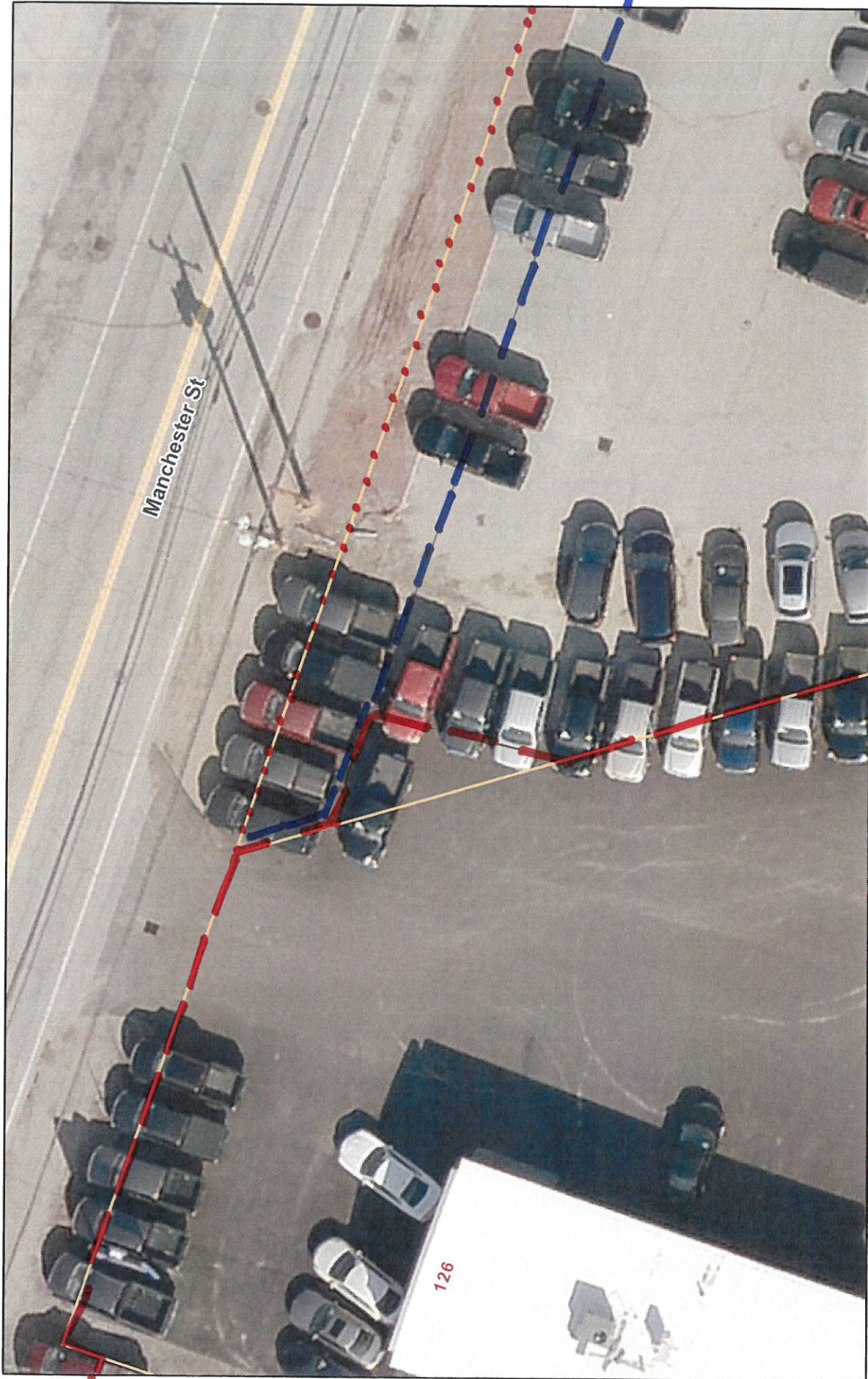
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0.01 Miles

0.005

0.0025

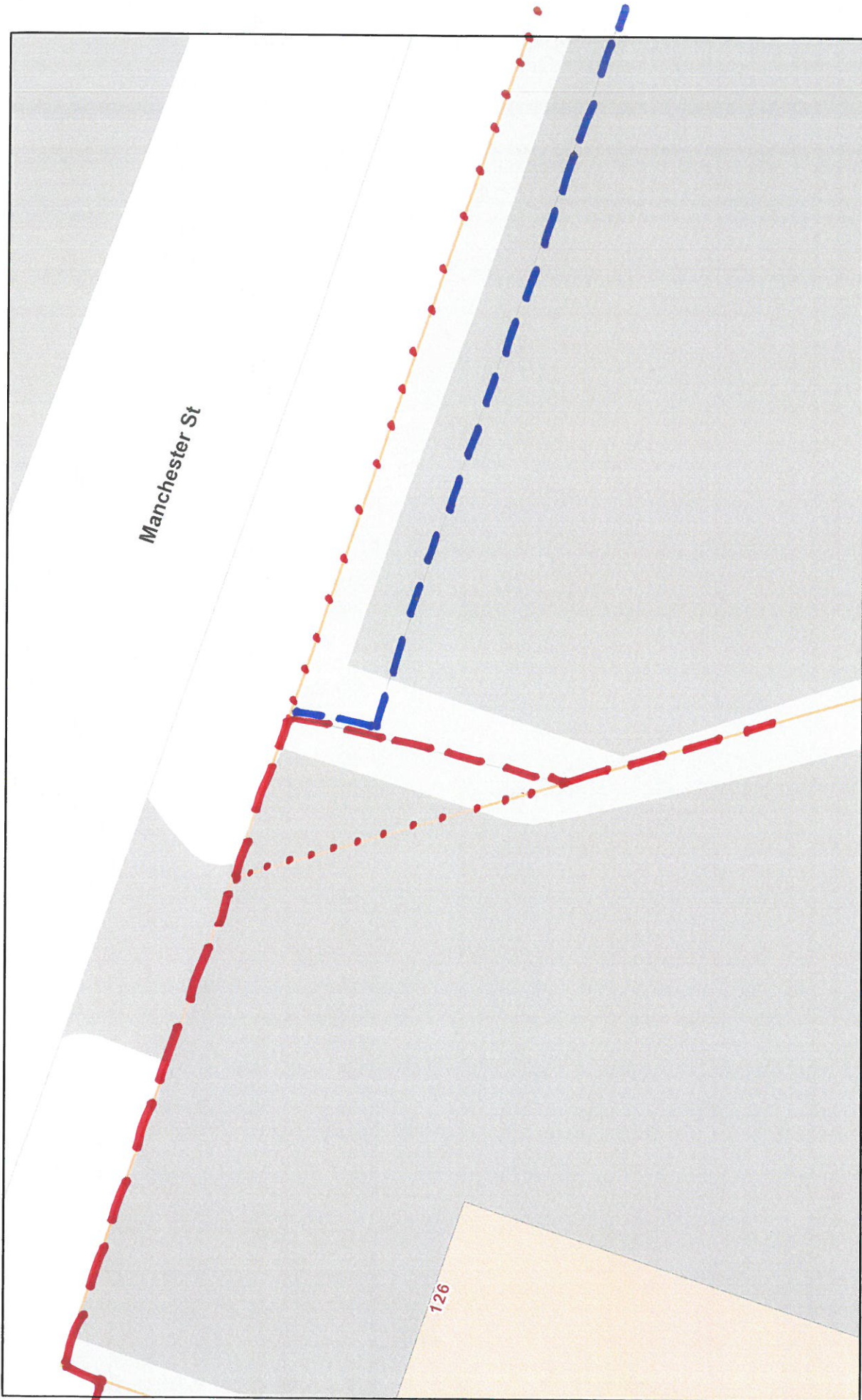
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December 20, 2021

1:240





Manchester St

126

December 20, 2021

1:240

40 Feet

20

10

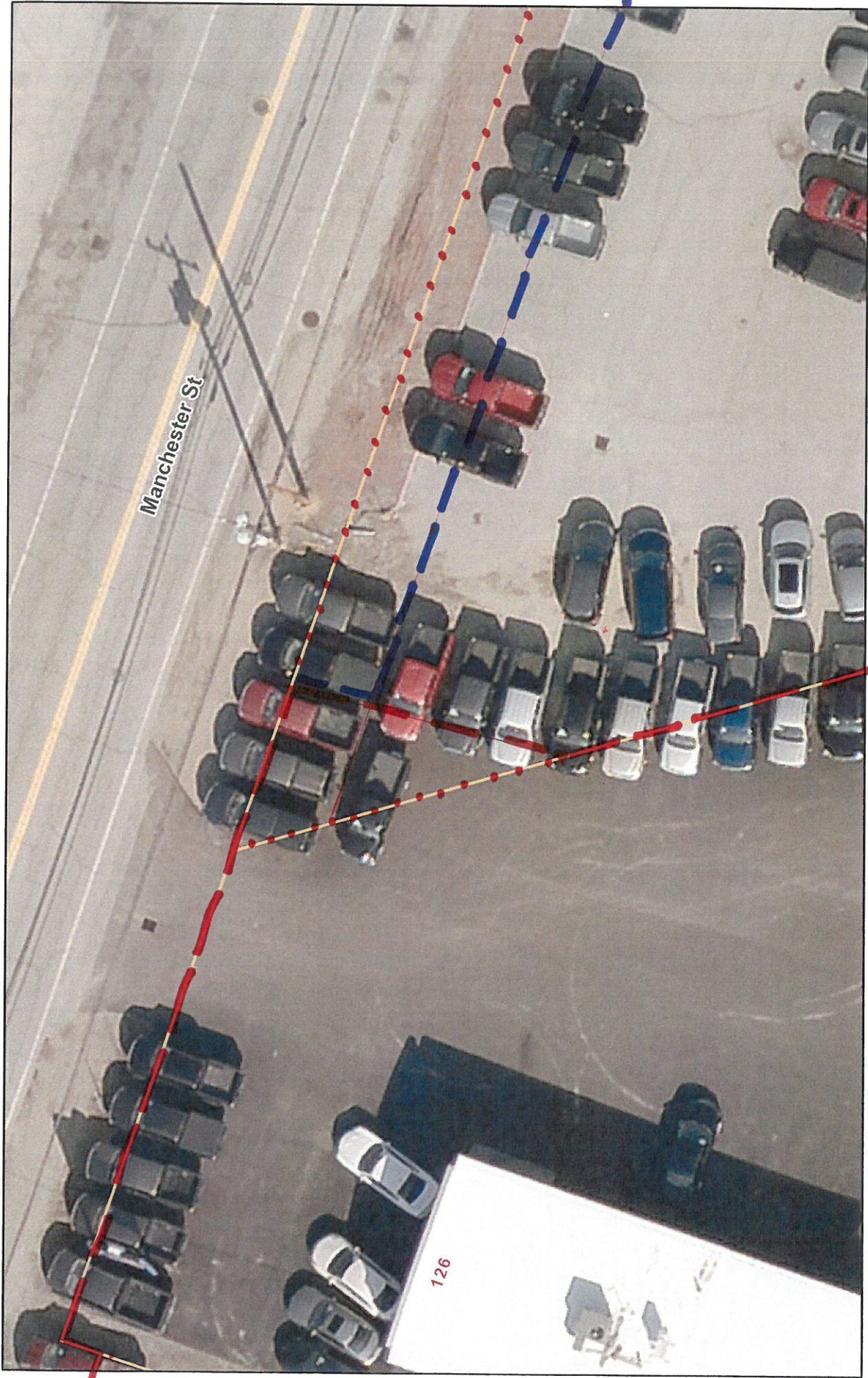
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December 20, 2021

1:240

